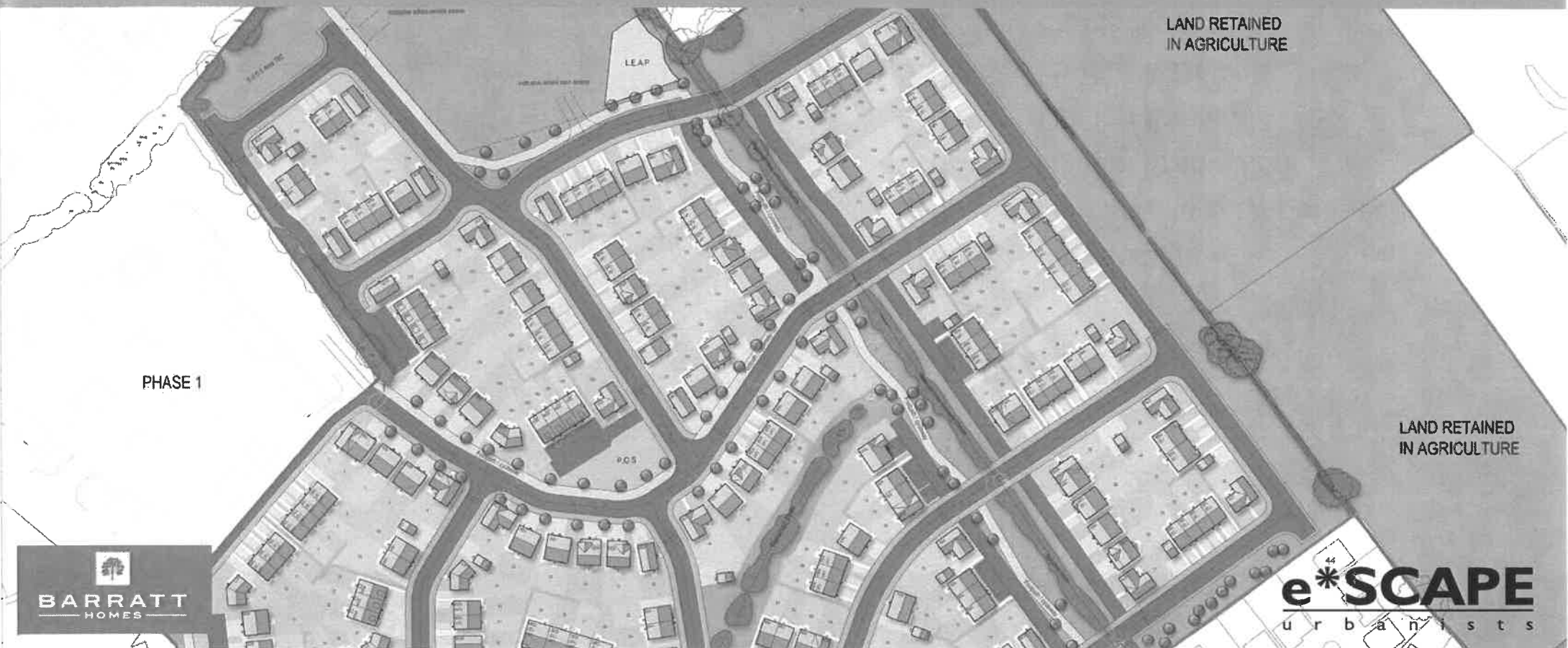


Design Statement

October 2018

Chipping Lane, Longridge (Phases 2 & 3)



Contents

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|------------|-----------------------------|
| 1.0 | Introduction |
| 2.0 | Constraints & Opportunities |
| 3.0 | Spatial Framework |
| 4.0 | Design Evolution |
| 5.0 | Layout & Design |
| 6.0 | Landscape |
| 7.0 | Sustainability |
| 8.0 | Summary & Conclusions |



Introduction

This document has been produced on behalf of Barratt Homes in support of an application for residential development on land East of Chipping Lane, Longridge.

The application is for the approval of reserved matters (layout, scale, appearance and landscaping) for a Phase 2 & 3 development pursuant to outline consent 3/2014/0764 granted on 29 October 2015 and comprises the erection of 184 dwellings.

Two reserved matters applications have been submitted pursuant to this outline scheme (of which the latter was a re-plan). Reserved matters approval for the Phase 1 development comprising the erection of 118 dwellings (Ref: 3/2016/1093) was granted in September 2016. Approval was subsequently granted for a revised scheme in September 2018 for 124 dwellings ((Ref: 3/2018/0404).

This design statement presents an overview of the development proposals and demonstrates how the design concept has evolved from a thorough understanding and appreciation of the site's constraints, opportunities and context. It demonstrates how the development has responded to the design principles and parameters established by the outline planning consent and illustrates how feedback received during the pre-application process has been taken on board and has influenced the final design outcome.

This design statement should be read in conjunction with the range of documents and plans which have been submitted in support of the planning application.

The Development Proposals

The Application seeks reserved matters approval for layout, scale, appearance and landscaping for 184 dwellings. The housing mix will comprise 55 affordable homes, together with 129 open market dwellings (including elderly provision), alongside associated open space, landscaping and access (including pedestrian and cycle access).

The proposed development will include a mix of two, three and four-bedroom properties. Further details of which are provided in Section 5 of this Statement.

The boundaries of the application site are illustrated on Figure 01.01 opposite.

Content & Structure

The remainder of this document is structured as follows:

Section 1: Provides a description of the site and context.

Section 2: Describes the key constraints and opportunities presented by the site and which have informed the design outcome.

Section 3: Describes the design concept and demonstrates its compliance with the indicative masterplan presented within the outline planning application.

Section 5: Describes how the design of the development has evolved through a process of consultation with the local planning authority.

Section 4: Presents the proposed site layout and detailed elements of the scheme's design including street hierarchy and design, housing mix, built form and scale, parking, materials, appearance and detailing.

Section 6: Presents the proposed landscape strategy for the site.

Section 7: Set's out Barratt Homes' approach to sustainability.

Section 8: Presents our summary and conclusions.



The Site

The site is located on the northern edge of the town of Longridge within the administrative area of Ribble Valley Borough Council.

The site extends to 10.56 hectares and this phase relates to land directly to the east of Phase 1 (which is currently under-construction). The site is currently existing farmland used for pasture. The site comprises of pastoral fields dissected by field boundary hedgerows and hedgerow trees broadly surrounded by open countryside to the north and existing residential and commercial development to the south.

Topographically the site has localised undulation, with ground levels rolling gently north-west to south-east.

Accessibility

The site is within a few minutes walk of the local supermarket, wider town centre, primary schools and a number bus stops. The centre of Longridge is about a 6 minute walk from the site and provides a range of local shops and facilities.



Constraints & Opportunities

Figure 02.01 opposite illustrates the main physical characteristics of the application site and identifies the key development constraints and opportunities. It also examines the site's relationship with its immediate surroundings and adjacent land-uses and draws on the findings of a range of supporting surveys and studies submitted as part of this application. This analysis has informed the spatial framework presented in the next chapter which upon which the detailed site layout has been based.

The key site features are summarised below:

- **Access** - the site does not currently benefit from vehicular access. Vehicular access to Phases 2 & 3 will be provided from Phase 1 linking to the site access junction on Chipping Lane. Potential exists as part of the development to create new footpath/cycle connections between the site and Redwood Drive to the south.
- **Topography:** The site slopes gently from east to west by approximately 10 metres and by an average of around 5 metres from south to north. The lowest part of the site is located in the north-west corner which lies at an average height of around 105.0 AOD. Gradients are relatively gentle towards the south-west corner of the site, becoming steeper towards the north-western boundary. Remnant drainage ditches located at the base of hedgerows, all of which drain down to Higgin Brook.
- **Rights of Way** - no existing Public Rights of Way across the site.
- **Trees** - Existing trees on site are confined to the site boundaries and internal hedgerows. The majority of these have been identified as either Category B (moderate quality or value) or C (low quality or value). Two category A trees have been identified along the northern boundary that will be retained. In order to provide access for emergency vehicles along the southern site boundary, the existing Oak tree (Category A) will be removed and replaced with new specimen tree planting elsewhere within the site.
- **Hedgerows** - Internal field boundaries and the site's eastern boundary with Phase 1 are defined by existing mature hedgerows. Hedgerow G5 along the south-east boundary will be removed in order to accommodate dwelling driveways, pavements and usable garden areas. Hedgerow G4 to the north will be largely retained, with only selective trimming back to accommodate driveway access. Hedgerow G12 runs through the centre of the site along a north-south axis and will be retained largely intact although partial removal will be required to accommodate proposed internal roads. The remnant hedgerow extending diagonally across the site will again be remain with only partial removal to accommodate access roads. Hedgerow G14 along the site's eastern boundary will be retained and enhanced to create a buffer between the development and the retained agricultural land.
- **Watercourses** - Higgin Brook issues from a culvert adjacent to Sainsbury's service yard just to the rear of the properties fronting Inglewhite Road. It flows in a north-easterly direction along an established hedgerow marking the site's northern boundary before dog legging north-west following another hedgerow until it disappears into a culvert below the Cricket Club and drains off-site to the north, following the course of Longridge Road.
- **Views** - Due to the topography a number of views can be had into and out of the site. Long distance views to and from the site can be had from the Forest of Bowland. Mid-distance views can also be had to and from Longridge Fell. Localised views can be had into and out of the site from the rear of existing residential properties on the northern fringes of Longridge. This edge is somewhat degraded by the varied boundary treatments to the rear of properties, conservatories and garden sheds etc.
- **Residential Interface** - Residential uses adjoin the site's southern boundary with the rear gardens of existing houses along Redwood Drive backing onto the site. The development proposes a generous green buffer along this boundary in order to minimise any potential amenity impacts on existing houses.



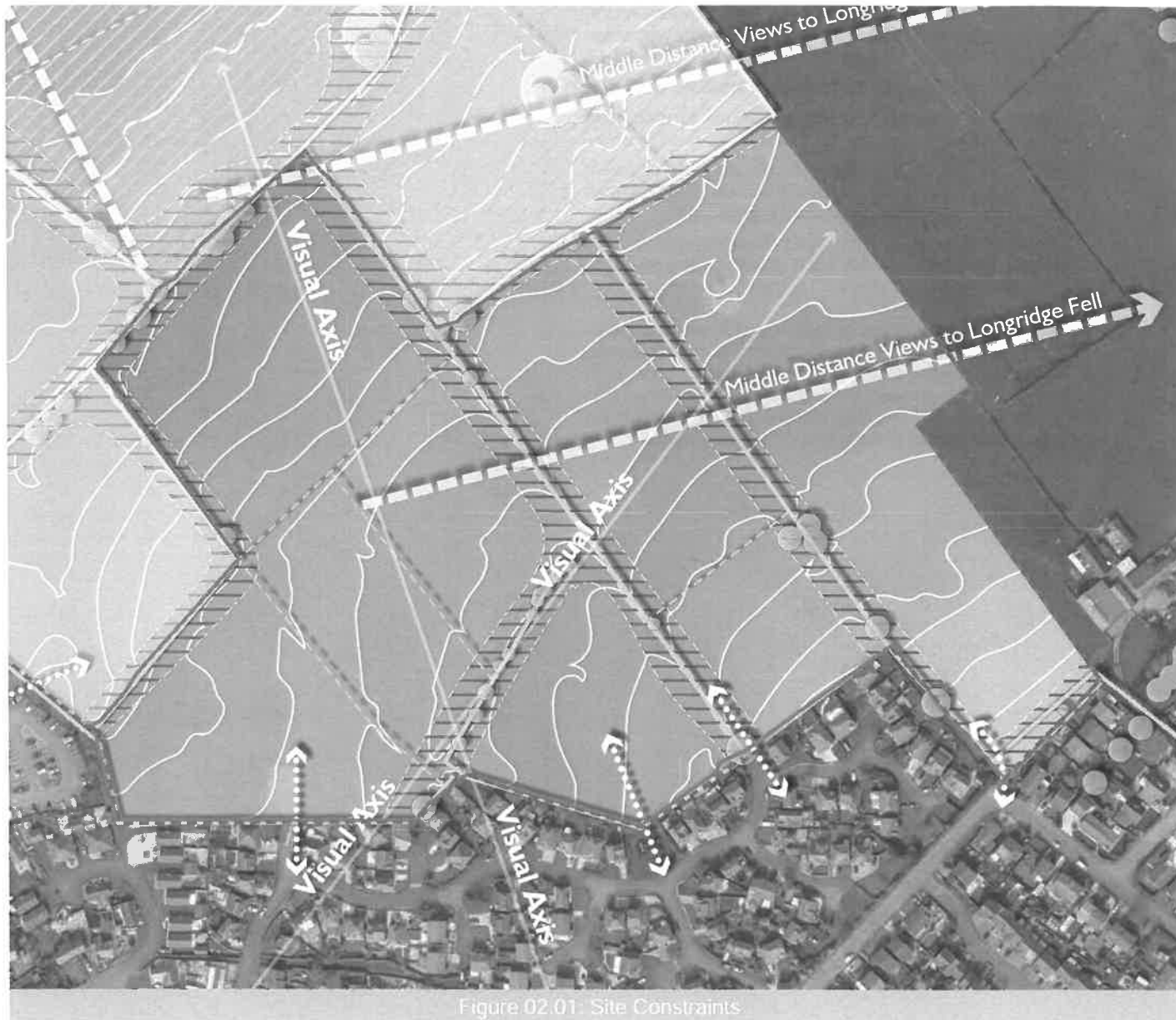


Figure 02.01: Site Constraints

Design Parameters

The current proposals have been developed in line with the principles established by the Parameters Masterplan (drawing ref. 013-008-P017 Rev E) which accompanied the outline planning application (presented opposite) along with the associated Parameters Landscape Strategy, Design Code and Design and Access Statement (dated August 2014).

Key elements include:

- The provision of 184 new dwellings comprising a mix of housing types and sizes to cater for local needs and delivering a total of 308 homes across all Phases (55 less than the upper limit of 363 set by the Outline consent).
- Primary access from Chipping Lane via Phase 1, with secondary pedestrian and cycle access points through to Redwood Drive;
- A clear and legible street hierarchy comprising a main Avenue through the centre of the site and series of secondary streets, shared surface lanes and features of public realm set within the streetscape;
- Strong frontages and natural surveillance throughout, most notably in relation to areas of public open space, footpath links and routes;
- Landscape buffer between existing housing to the south and the proposed development with generous offsets between new and existing houses;
- An organic layout with gently curving streets, a variety of development block shapes and sizes set within a strong green infrastructure network;
- An outward-facing development with houses orientated to benefit from attractive views over the surrounding countryside;
- Generous open space provision including both formal and informal spaces;
- Retention/enhancement of existing hedgerows and trees within and around the edges of the site where possible;
- Recreational footpath and cycle linkages throughout the development linking to public open space and play provision;
- A landscape buffer to the east of the site to filter views of the development and create a softer transition between built form and the open countryside;
- Provision of a strong network of green corridors through the site to provide visual relief to the built form, provide pedestrian routes and corridors for the movement of wildlife;
- Retention of existing land to the east for agricultural land;
- Provision made for a new Primary School at north-west corner of the site;
- Provision of landscape buffers throughout the development most notably to the south, east and north to offset between built development and the open countryside; and
- Recreational footpath and cycle linkages through the development linking to public open space and play provision.

Figure 03.02 below shows how these principles have been incorporated into the layout and structure of the development. Whilst minor changes to the outline parameters have been required in order to respond to technical and other constraints that have emerged during the detailed design process, the underlying principles as detailed here remain unchanged.

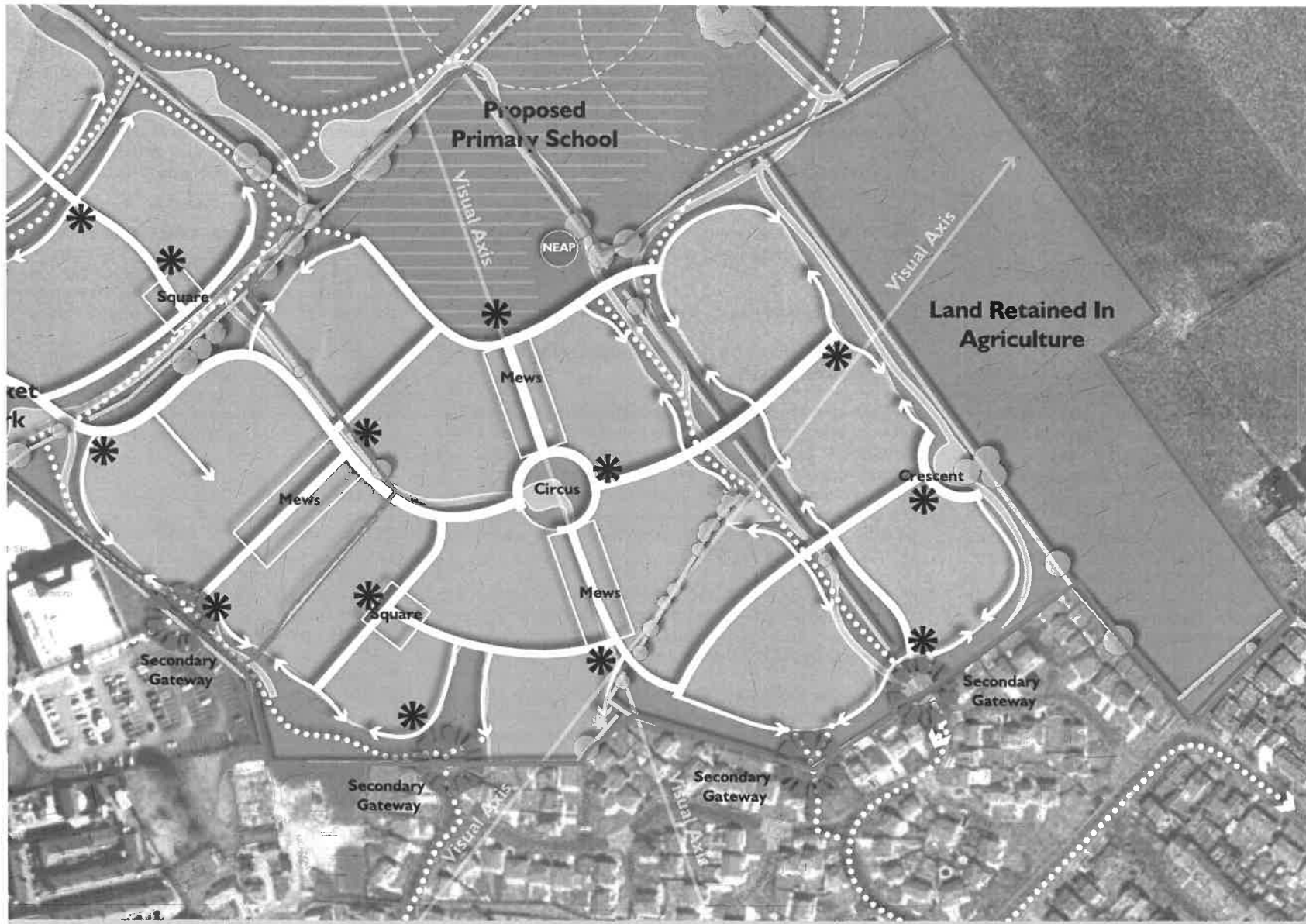


Figure 03.01: Outline Parameters

Spatial Framework

The parameters plan opposite (Figure 03.02) illustrates the spatial framework underpinning the site layout. It shows the proposed movement pattern/street hierarchy, green infrastructure network, block form/urban grain and density. These elements have created the basic structure around which the detailed site layout has evolved.

The spatial framework has been informed by the design parameters approved at outline stage as set out above. Only minor amendments to the road layout, block structure and landscape strategy have been required in order to respond to technical and other constraints as part of the detailed design process.

Movement Hierarchy

The parameters plan shows a simple movement hierarchy based on an interconnected network of avenues, streets, shared-surface lanes, shared drives and footpaths. The arrangement of streets and other routes within the site is designed to ensure good connectivity with the surrounding street network, provide logical and efficient routes through the development and ensure a layout which is clear and legible.

The main avenue and principal route through the development enters the site from Phase 1 along the western site boundary and provides a connection onto Chipping Lane. Trees within private front gardens either side of the Avenue provide an attractive tree-lined route through the development. Block pavers are introduced at a key intersection along the avenue as a feature of public realm and to signal to drivers that they are entering a slow-speed residential environment. The Avenue is flanked by an area of public open space to the north that acts as a focal point within the development, providing amenity space for play and recreation.

A series of secondary streets and shared-surface lanes extend off the main avenue to provide access to properties to the north and south. A small number of units will be accessed off shared driveways which will serve a maximum of four dwellings.

The central green corridor is flanked along both sides by shared-surface lanes and drives along its length, providing a soft transition between the open space and the surrounding built form. This arrangement also enables properties to front directly

onto the open space, providing good surveillance and enclosure.

A footpath connection is provided along the southern boundary that links with the footpath network within Phase 1 and also provides access onto Redwood Drive. A footpath/cycle link is also provided the central green corridor, linking the proposed school site with Redwood Drive.

Future access and egress is provided to the proposed school site at the north-east corner of the site.

The street pattern has an organic form with gently curving streets reflecting the landscape setting and surrounding built character.

Block Form and Layout

Figure 03:02 illustrates the general principles of block structure and layout within the site.

An urban block structure is proposed with houses facing onto the street providing natural surveillance and active frontages. In this arrangement, private gardens are located to the rear providing security, privacy, and good separation between units. It also avoids the front of properties overlooking the rear of buildings. The blocks are large enough to accommodate good sized rear gardens which provide opportunities for property extensions, alteration and conservatories.

Houses are orientated to provide surveillance to surrounding streets and spaces and to benefit from views towards the open countryside to the east. The central green corridor is flanked by houses along its full length, providing surveillance and enclosure.

Houses have been set back from the southern boundary to enable a generous landscape buffer of between 8 and 30 metres to be provided along the site edge that will help to maintain the privacy and outlook of existing properties along Redwood Drive. This is consistent with the parameters established by the Design Code approved at the Outline stage which required a buffer of between 5m and 40m.

Green corridors are provided through the centre of the site following the alignment of existing field boundaries and will help to: provide visual relief to the built form; enhance on-site biodiversity; create views through the development; and improve permeability for pedestrians.

The built form is concentrated in the central and western parts of the site to allow land to the east to be retained in agricultural use and to enable the retention of the existing hedgerow and trees along this boundary and the provision of an appropriate landscape buffer of between 12-13.5 metres wide (excluding the retained agricultural land) to filter views of the development from the wider countryside. This accords with the approved Design Code which required an eastern landscape buffer ranging in width from 12m to 25m (the upper limit extending into the retained agricultural land).

The built has been pulled back from the northern boundary where a series of balancing ponds are proposed as part of the sustainable urban drainage strategy for the site. A landscape buffer will be provided between the road edge and the proposed school site measuring between 12 and 22 metres in width.

Any potential for unacceptable overlooking or overshadowing has been minimised with good standards of amenity provided for future occupants. The configuration and size of development blocks has been designed to enable minimum back to back distances of 21m to be achieved.

Amount

The application site is 10.56ha (gross) in total of which approximately 4.84ha is developable land for new housing. Total undeveloped land totals 5.72ha including areas of public open space, green buffers and corridors, land set aside for sustainable urban drainage infrastructure, retained agricultural land and the proposed school site.

It is proposed that 184 homes can be accommodated on the site at a net average density of 38 units per hectare.

The number of dwellings proposed as part of this reserved matters application is in accordance with the parameters of the outline planning permission which granted consent for the erection of up to 363 dwellings (as an upper limit). Taking into account the 308 dwellings proposed within Phase 1, the current phase will result in a combined delivery of 308 dwellings, 55 dwellings less than what was originally permitted as part of the outline planning permission.



Figure 03.02: Spatial Framework Plan

Density

The proposed density (net) will be 38 dwellings per hectare and the building heights will not exceed 2.5 storeys. This approach and provision is consistent with the approved Design and Access Statement which envisaged a density of circa 39 dph, which is reflective of the local area.

Density is distributed evenly across the site with mix of detached houses, semis and short rows of townhouses spread throughout the development.

The proposed density is considered entirely appropriate given the site's setting and the character of surrounding villages. The proposed density allows for the retention and provision of significant green infrastructure on the site, creating a sense of openness and space within the development. An appropriate balance is achieved between the need to reflect local character, make efficient use of a consented site in a sustainable location and provide for a range of housing needs.

Open Space

Extensive landscaping is proposed throughout the site, as detailed in the landscape strategy. The development framework ensures that the development sits within a strong landscape setting which serves not only to provide a high level of on-site amenity, but also protects the amenity of neighbouring uses, enhances views into the site from the surrounding countryside, and provides other benefits in terms of supporting biodiversity and habitat creation. The extent and type of public open space and public realm to be provided has been designed to ensure that high quality open space is provided which is located within accessible locations throughout the development in accordance with the parameters established through the outline planning permission.

A green corridor of between 16 and 18 metres is retained through the centre of the site along the line of the existing field boundary and north-south ditch. The existing belt of planting will be retained and enhanced with additional tree planting and native tree and shrub whip planting. A pedestrian/cycle route is provided through the corridor which will also provide ecological and recreational benefits with the provision of trim trail play equipment and areas

of usable open space. A secondary green corridor extends off the main space in a south-westerly direction along the existing belt of trees, connecting with the green buffer along the site's southern boundary.

Towards the north-west corner of the site, a LEAP is proposed adjacent to the school site at the northern terminus of the central green corridor. Its location in relation to the school and pedestrian routes through the development will ensure safe and easy access by residents as well as ensuring good surveillance from surrounding properties.

A central open space is provided along the northern edge of the Avenue and will serve as a key focal point within the development. This will be designed as multi-functional space catering for a range of different needs. It will comprise largely of amenity grassland with seating areas and is bordered by hedgerow planting and trees flanking the Avenue. The surrounding built form is designed and orientated to provide surveillance and enclosure to the space.

Additional open space is provided around the edges of the development in the form of landscape buffers, undeveloped agricultural land and drainage areas.

A mixture of hard and soft landscaping including trees, hedges, shrub planting, climbers, bulbs and wildflowers is proposed to ensure that the proposed development responds accordingly to the established design principles. Existing trees will be retained where possible, with landscaping and planting proposed throughout the site, as identified within the accompanying Landscape Strategy.

Access

The primary vehicle access into the site enters along the western boundary from Phase 1 linking to the site access junction on Chipping Lane, approved as part of the outline planning application. This access has subsequently been constructed as part of the Phase 1 development.

A series of pedestrian gateways along the southern boundary connect the site with Redwood Drive and the wider community. Pedestrian access into the site can also be gained from Phase 1 along the western boundary.

Emergency access will be provided to the south of the site connecting to Redwood Drive. The access will be in the same location as the pedestrian and cycle access at the southern end of the central green corridor.



Design Evolution

The final layout is the product of a considered process of review and refinement over a period of months that has resulted in a high quality design outcome that responds to the constraints and opportunities presented by the site.

The image opposite shows an early concept design that was presented to Officers at Ribbles Valley Council in a pre-application meeting in August 2018, attended by Barratt Homes and their representatives. Feedback received through this consultation process was taken on board and resulted in a number of changes to the design, as highlighted opposite and below.

Pre-Application Discussions

The Applicant has engaged in extensive pre-application discussions with the Council both as part of the original outline permission and the reserved matters applications.

In relation to this reserved matters application, a meeting was held with the Council on 20 August 2018 which provided an opportunity for the Applicant to outline their emerging proposals for the site (see Figure 03.04 opposite) and to agree the scope of the reserved matters submission. A formal written response was received on 29 August 2018, setting out the Officer's initial views on the scheme and list of application validation requirements.

Issues identified for further consideration included the need to:

- revisit the use of 'rear-curtilage tandem parking' arrangements on some plots;
- ensure levels of parking provision accord with current standards;
- provide dedicated refuse storage and bicycle storage provision for the apartment blocks;
- reduce the visual dominance of parking along the streetscene in some locations;
- ensure appropriate access arrangements to the potential school site;
- provide an appropriate landscape buffer along the southern site boundary that accords with the parameters established by the Design Code approved at the Outline stage (which required a buffer of between 5m and 40m);

- provide an appropriate landscape buffer ranging from 10m to 45m along the northern edge overlooking the school land and proposed cricket pitch incorporating the provision of informal pathways through trees and meadow planting.
- provide an eastern landscape buffer ranging in width from 12m to 25m to accord with the approved Design Code;
- revisit the street hierarchy to ensure the provision of a village avenue, distinct character areas, public realm and focal features within the proposed layout;
- strengthen the central green corridor running north to south through the site through additional tree, copse and woodland planting, inclusion of trim trail equipment, a more naturalised setting for the footpath/cycleway and the inclusion of 'nodes' and transitions in public realm treatment;
- provide a series of secondary gateway connections to the existing community to the south, including a linkage with the existing play area on Redwood Drive;
- ensure a minimum back to back interface distance of 21m and avoid issues of overlooking between properties;
- ensure a minimum rear garden length of 10.5m; and
- provide an appropriate level of usable public open space, possibly in the form of a network of small usable spaces, or partially in the form of an informal trim trail with a number of smaller pocket parks/spaces scattered throughout the development.

In response, a number of changes were made including:

- Changes to the housing mix including the removal of apartment buildings;
- minor changes to road layout and block configuration;
- introduction of avenue trees and sections of paved carriageway to create stronger definition and legibility to street hierarchy;
- central green corridor redesigned with introduction of paved lanes/drives along both sides, paved thresholds at road crossings, reduced linearity of pedestrian/cycle link and provision of trim trail equipment;
- landscape buffer along southern boundary increased in width to between 8 and 30m with additional tree planting, a pedestrian link and trim trail equipment introduced. Houses set back further from site boundary to north of road to reduce

potential for overlooking and amenity issues;

- provision of a landscape buffer of between 12 and 22 metres between the development and the cricket pitch and primary school site that will include native hedgerow planting and areas of scattered native tree planting to filter views;
- secondary pedestrian gateways introduced along southern boundary, connecting the site with Redwood Drive and the wider community-;
- a generous landscape buffer (12-13.5m) introduced along the eastern boundary and interface between the development and retained agricultural land; and
- access and egress arrangements to school site shown on revised layout.

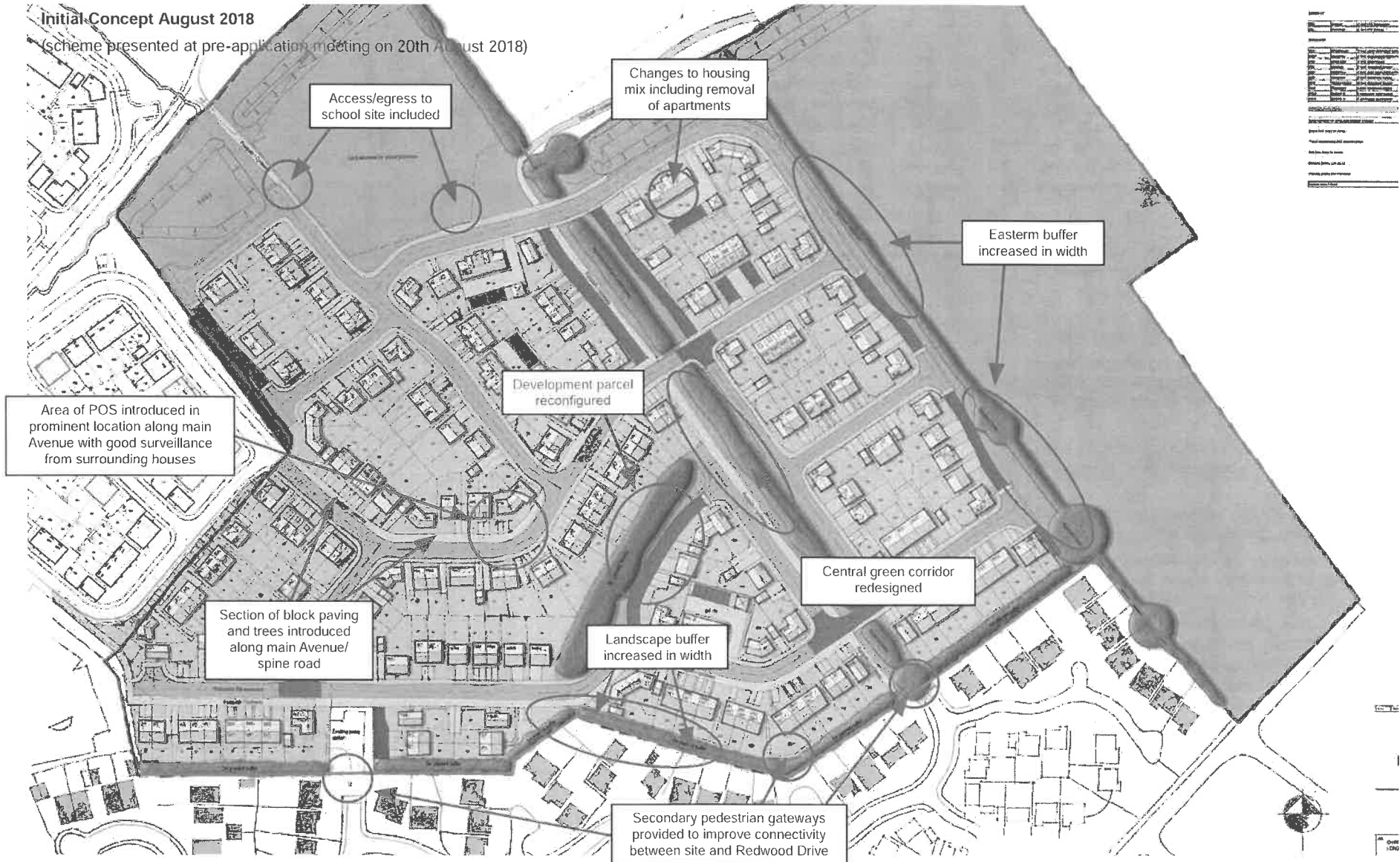


Figure 04.01: Summary of Changes to Early Concept Design

Layout

Figure 05.01 opposite illustrates the proposed site layout which builds on the framework provided by the design parameters above and responds to the principles established by the illustrative masterplan that accompanied the outline planning application.

The following section describes detailed elements of the proposed development including housing mix, car parking, scale and massing, street hierarchy, appearance, architectural detailing, building materials and landscape.

The proposed layout is characterised by:

- A variety of house types and sizes to cater for local need and ensure a balanced and cohesive community;
- an organic structure of discreet, human-scale development parcels in an attractively landscaped and naturalistic setting;
- an urban block structure with houses facing onto the street providing natural surveillance and active frontages with private gardens located to the rear providing security and privacy;
- a clear hierarchy of open spaces including a series of green corridors, landscape buffers, equipped play areas and general amenity areas;
- careful positioning of buildings to maximise views and surveillance of public open spaces;
- a highly legible structure of streets and where residents and visitors can intuitively find their way around the development with good connectivity onto surrounding streets;
- a strong sense of rhythm and continuity along streets with houses set along a relatively consistent building line behind small front gardens, with subtle staggering or minor setbacks to provide variety and interest or to define a junction or space or act as a traffic calming device;
- good separation between facing units with overlooking distances to the frontage of properties of approximately 20 metres on average along principal streets. A minimum back to back distance of 21 metres is maintained wherever possible. If this rear distance is reduced the properties are usually at an oblique angle to one another or they relate to specific urban design situations;
- good pedestrian connectivity with the surrounding area with linkages into Phase 1 and onto Redwood Drive and Chipping

Lane;

- an efficient arrangement of streets and spaces which makes effective use of an irregular shaped site;
- mid-range and long distance views towards surrounding countryside maintained along streets and between houses and from publicly accessible green spaces;
- generous setbacks and landscape buffers to adjoining residential areas to protect the amenity and outlook of existing properties;
- an outward-facing development with houses orientated east towards the retained agricultural land the wider countryside; and
- use of corner-turning and dual aspect houses at key intersections to provide definition and surveillance to streets and spaces.

Key Design Features

- 1 Tree-lined avenue provides main access and spine road through the development;
- 2 Block paving introduced to carriageway as gateway feature and traffic calming device;
- 3 Multi-functional area of public open space creates focal point within the development along the primary spine road;
- 4 Central green corridor follows line of existing tree belt and accommodates trim trail equipment and a pedestrian/cycle link through the site;
- 5 Block paving introduced where routes cross central green corridor;
- 6 LEAP located adjacent to school site at northern terminus of central green corridor to maximise accessibility by residents;
- 7 Landscape strip provides physical and visual buffer between the development and retained agricultural land and wider countryside to the east;
- 8 Series of balancing ponds along northern boundary form part of the sustainable urban drainage strategy for the site;
- 9 Pedestrian/cycle link set within landscape buffer along southern boundary provides link into Phase 1 as well as providing access onto Redwood Drive;
- 10 Landscape buffer overlooking the school site and cricket pitch land;
- 11 Outward-facing development with houses orientated to benefit from attractive views across retained agricultural land and open countryside beyond; and
- 12 Land allocated for school provision.



Figure 05.01: Site Layout Plan

Housing Mix

A mixture of 2, 3 and 4 bed properties are proposed, which is consistent with what was approved as part of the outline planning permission.

As a proportion of the total mix, the number of houses in each size category is as follows:

- 2 bed - 27%
- 3 bed - 50%
- 4 bed - 23%

The housing mix for this site comprises a total of 13 different house types, each specifically tailored to the site.

The mix includes townhouses, detached houses and bungalows.

The proposed housing mix is summarised in table 05.02 opposite. It identifies different house types incorporated into the scheme, the number of bedrooms the house type has, and the amount of each house type included, as well as an overall total.

A total of 7 (30%) affordable houses will be provided, including both social rent and intermediate housing.

Core Strategy Policy DMH1 requires that 15% of the overall housing provision (for the development) should be solely for the elderly and of this 50% should be affordable (7.5%). In total, 28 elderly homes are to be provided, comprising 2 bed bungalows and 2 bed houses. Of these 28 elderly homes, 14 properties will be affordable and comprise 7 x 2 bed bungalows and 7 x 2 bed houses. In addition, 41 other affordable homes will be provided, comprising 21 x 2 bed mews and 20 x 3 bed mews, resulting in 55 affordable homes.

SCHEDULE OF ACCOMMODATION

| Ref | Barratt Type | House Type | Sqft | No | Total Sqft |
|---|--------------|---------------------------------------|------|------------|---------------|
| AFFORDABLE TYPES | | | | | |
| STANDARD | | | | | |
| KEN | Kenley | 2 Bedroom affordable mews | 624 | 21 | 13104 |
| BRN | Brandon | 3 Bedroom affordable mews | 706 | 20 | 14120 |
| ELDERLEY | | | | | |
| BED | Bedale | 2 Bed LTH Bungalow | 594 | 7 | 4158 |
| BEL | Belmont | 2 Bed LTH House | 731 | 7 | 5117 |
| AFFORDABLE TOTAL | | | | 55 | 36499 |
| PRIVATE SALE TYPES | | | | | |
| ELDERLEY | | | | | |
| BED | Bedale | 2 Bed LTH Bungalow | 594 | 7 | 4158 |
| BEL | Belmont | 2 Bed LTH House | 731 | 7 | 5117 |
| STANDARD | | | | | |
| MAI | Maidstone | 3 Bed semi-detached house | 830 | 26 | 21580 |
| MOR | Moresby | 3 Bed dual aspect semi-detached house | 854 | 18 | 15372 |
| KNS | Kingsville | 3 bed townhouse | 1072 | 14 | 15008 |
| LUT | Lutterworth | 3 bed detached house | 1001 | 5 | 5005 |
| ALD | Alderney | 4 bed dual aspect detached house | 1225 | 19 | 23275 |
| DER | Derwent | 3 bed detached house | 920 | 9 | 8280 |
| WIN | Windermere | 4 bed detached house | 1073 | 22 | 23606 |
| THO | Thornton | 4 bed detached house | 1202 | 1 | 1202 |
| HAL | Halton | 4 bed detached house | | 1 | 1267 |
| PRIVATE SALE TOTAL | | | | 129 | 123870 |
| Total number of units and square footage | | | | 184 | 160369 |

Figure 05.02: Schedule of Accommodation

Scale

Proposed building heights are illustrated on the plan opposite. In order for the development to sit well within the wider context, the maximum height of buildings will be two and a half storeys with most properties being 2 storeys.

Building heights have been carefully considered. Two and a half storey houses have been cited away from the sensitive interface with the retained agricultural land to the east and wider countryside beyond. Houses along this boundary comprise a mix of single and two houses to provide a varied roofscape and more natural edge to the development.

Two and half storey houses are instead concentrated primarily within the western part of the site, overlooking areas of public open space or set back from the site boundaries.

A number of single storey houses have also been located at the southern end of the site to minimise the visual impact on existing properties along Redwood Drive.

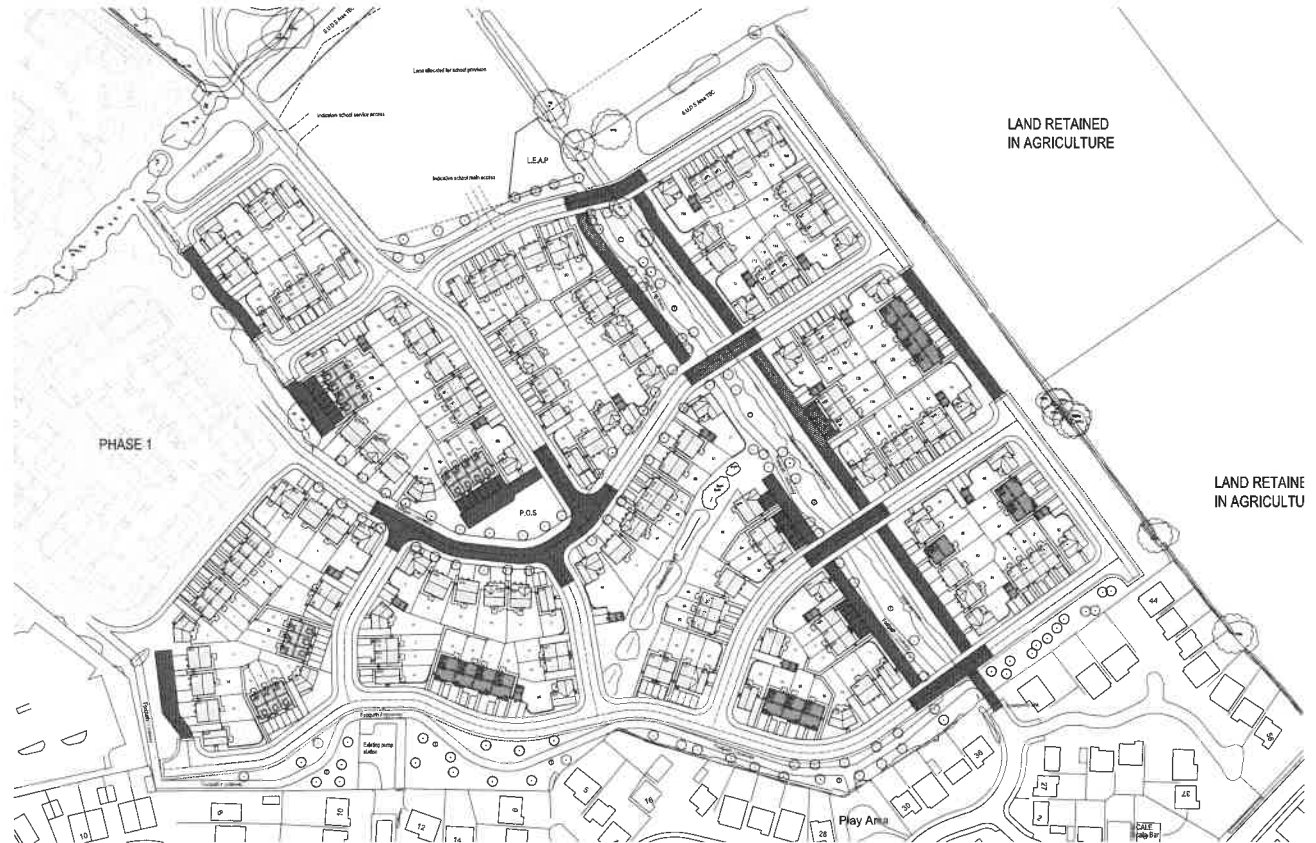


Figure 05.03 Building Heights



Parking

A balanced approach has been adopted which employs a range of parking solutions including on-street and on-plot (some to frontage, some to the side of properties and some garaged or integral). A summary of the parking typologies within the proposed development is illustrated in Figure 05.04 opposite.

All parking is provided close to, and visible from, people's homes offering security and convenience.

The proposed overall residential development will consist of 115 private houses, 41 affordable units and 28 elderly living units of which 14 will be affordable units.

The car parking strategy for Phases 2 & 3 has followed the same allocation process to the car parking strategy that was approved as part of the Phase 1 Reserved Matters application. Therefore, car parking spaces have been allocated to individual units using the following formula:

Affordable Dwellings:

- 2-bedroom Mews; 1 space per dwelling
- 3-bedroom Mews; 2 spaces per dwelling

Affordable Elderly Dwellings:

- 2-bedroom Bungalows and Houses; 1 space per dwelling

Private Sale Elderly Dwellings:

- 2-bedroom Bungalows and Houses; 1 space per dwelling

Private Sale Dwellings:

- 3-bedroom Townhouses and Houses; 2 spaces per dwelling
- 4-bedroom Detached Houses; 3 spaces per dwelling

The four-bedroom houses will be provided with a garage for one car with two spaces provided on the driveway. The lower number of parking spaces for the affordable units and elderly units reflects the lower car ownership levels among this group of people.

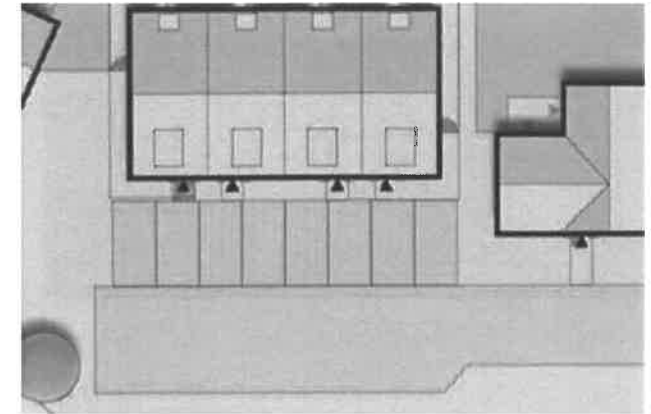
Based on the above formula there will be 61 parking spaces provided for the affordable units, 28 parking spaces provided for the elderly living units and 273 parking spaces provided for the

standard private sale units. This provision represents a parking ratio of 2.0 spaces per dwelling for Phases 2 & 3, which is the same as Phase 1.

Cycle Parking

The cycle parking strategy for Phases 2 & 3 has followed the same allocation process to the cycle parking strategy that was approved as part of the Phase 1 Reserved Matters application. This strategy considered that cycles will be stored within each unit.

1. Front Courtyard Parking



Design Principles:

Associated with townhouse rows

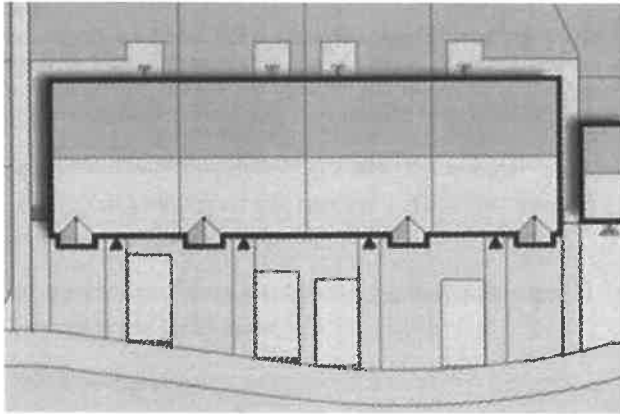
Tree planting and landscaped privacy strips to front of properties will assist in softening visual impact of parked vehicles

Long rows of parking bays avoided (limited to maximum of 8 spaces)

Block paving to parking bays



1. Frontage Parking



Design Principles:

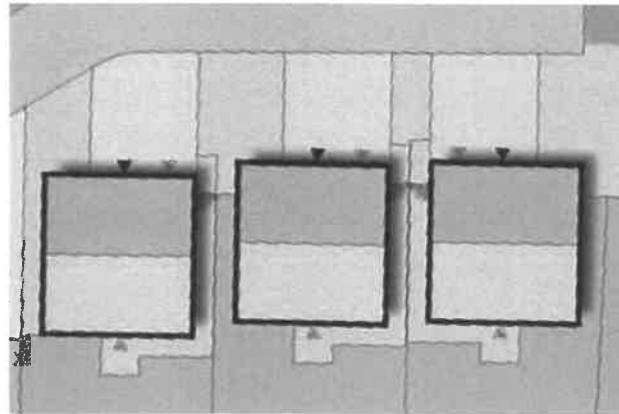
Tree planting and soft landscaping between adjacent parking bays to help provide screening to parked vehicles and soften visual impact

Long rows of parking bays avoided

Single parking bay dimensions: 4.8m x 2.4m



2. Integral Garages



Design Principles:

Typically associated with larger detached houses

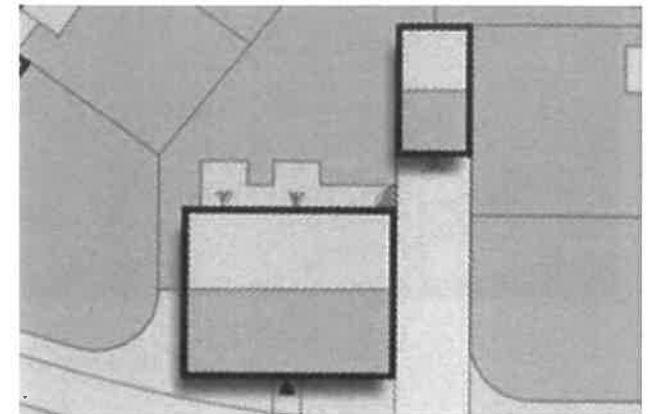
Additional parking provided in front of garage on private drive

Typically limited to single garage to avoid dominating ground floor of house

Glazed panels to garage doors to add visual interest



3. Side Parking (with and without garages)



Design Principles:

Parking on private drives to side of house or within single or double garages

Garages set back from the front of the house to reduce visual prominence along the street

Two adjacent drives avoided where possible to reduce breaks in continuity of building line along the street



Figure 05.04: Parking Approaches

Street Hierarchy

Figure 05.05 opposite illustrates the proposed street hierarchy which comprises a primary avenue, secondary streets, shared surface lanes and drives.

The detailed movement network is designed to create a permeable, logical and easily navigable development that provides direct routes from where people live to where they will want to go.

The scheme is very legible with a clear hierarchy of streets and spaces. The site is served by a main avenue which enters the site from Phase 1 along the site's western boundary and which forms a central spine road through the development. A series of secondary streets and lanes extend off this main route to serve development parcels to the north and south.

Legibility will be reinforced by the use and choice of different materials to distinguish primary and secondary routes with lanes for example being surfaced with block paving in contrast to the main streets which will be more traditional in form with an asphalt surface.

Several sections of road are designed as shared surface spaces with the use of block paving and flush kerbs helping to signal to drivers that they are entering a slow-speed environment where pedestrians have priority.

Streets are designed with gentle curves to limit sightlines, slowing vehicles and creating a more interesting environment for pedestrians. Streets and spaces are enclosed and well-overlooked by buildings. Good sized front gardens and architectural variety will contribute towards an attractive and interesting streetscene for pedestrians. Homes have generous glazing at ground floor level and provide good surveillance of streets helping pedestrians to feel safe and secure.

There is diversity in the mix and type of housing along the main streets to create individuality and a variety of experience as one moves through the development.

Each element in the hierarchy is described in more detail over the following pages.

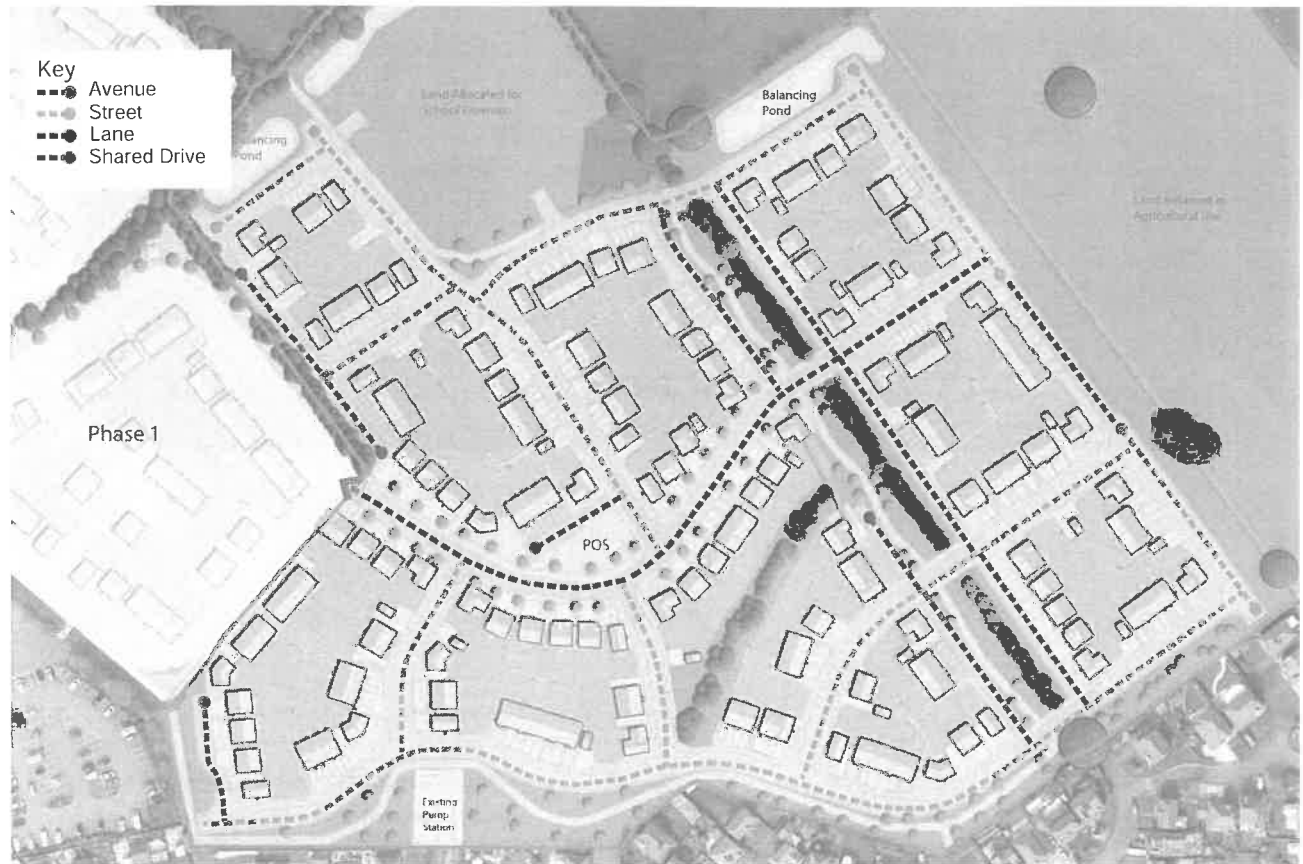
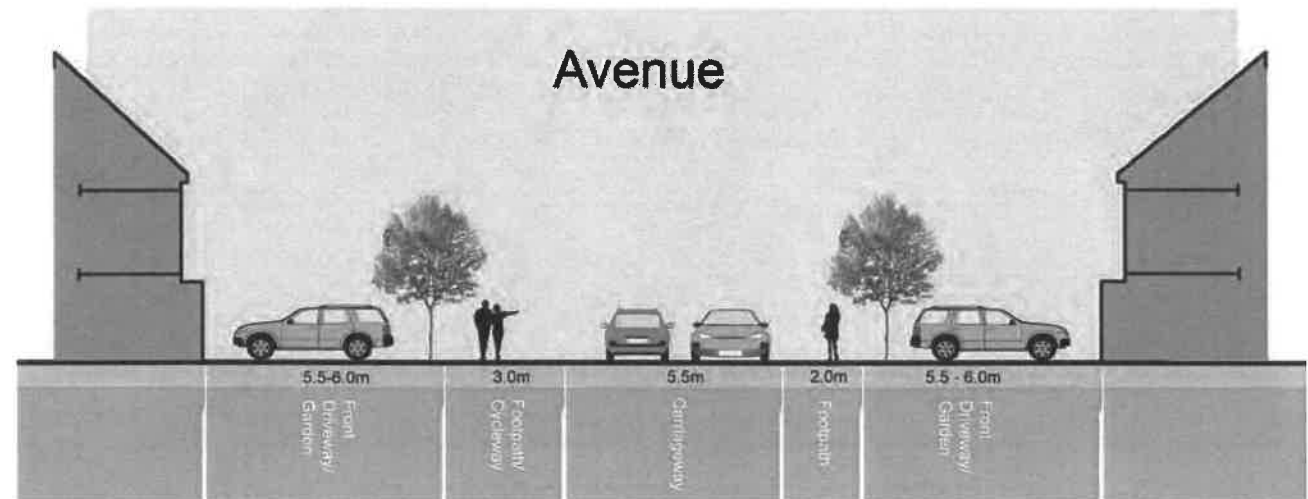


Figure 05.05: Street Hierarchy

| Criteria | Street Type: Avenue |
|-----------------------------------|--------------------------------|
| Dimensions & Character | |
| Carriageway Width | 5.5m |
| Footway | 2m Footpath/3m cycleway |
| Service Strip | Within footway |
| Direct Access to plots | Yes |
| Building Setback | Varies (typically 5.5-6m) |
| Design Speed | |
| Target Speed | 30mph |
| Design Details | |
| On Street Parking | Yes |
| Junction Radius | 6m |
| Traffic Calming | block paving threshold |
| Plot Boundary Treatment | Railings/Hedgerow/Shrubs/Trees |
| Materials | |
| Footway Surfacing | Black Tarmac |
| Carriageway | Black Tarmac |

Avenue - Design Principles:

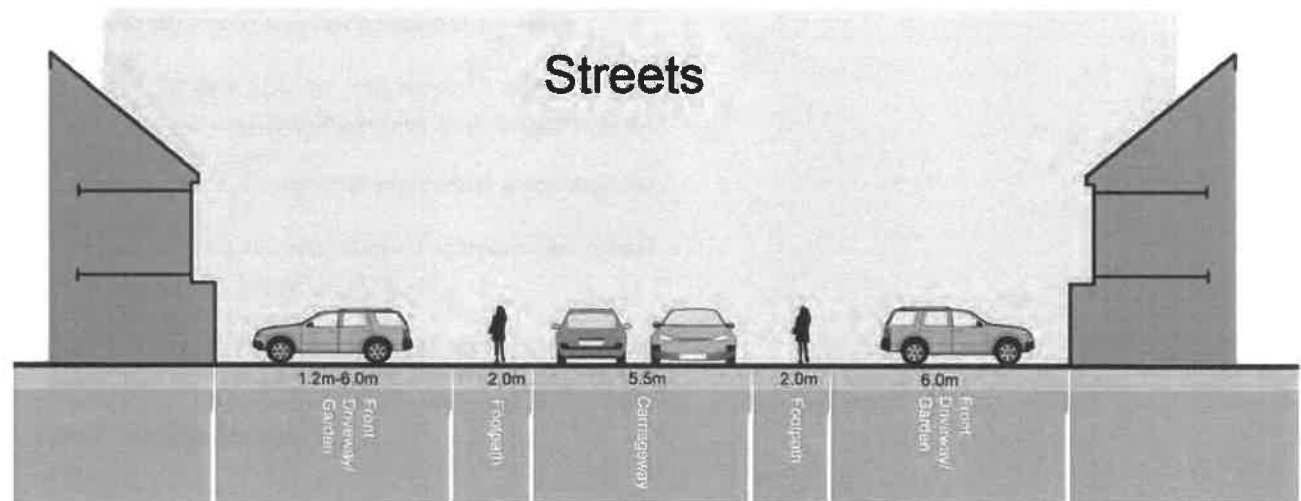
- Main route into the site from Phase 1 accommodating higher traffic flows through the development
- Clear definition between carriageway and footpath
- Surrounding built form comprises mostly two storey dwellings set behind front gardens/driveways
- Formal character with ball top railings to plot boundaries and regular spaced trees set within front gardens and adjacent open spaces
- Block paving introduced along central section as traffic calming device and to provide gateway feature
- Flanked by area of public open space to north



| Criteria | Street Type: Streets |
|-----------------------------------|---|
| Dimensions & Character | |
| Carriageway Width | 5.5m |
| Footway | 2m (one or both sides) |
| Service Strip | Within footway |
| Direct Access to plots | Yes |
| Building Setback | Varies (typically 1.2-6m) |
| Design Speed | |
| Target Speed | 30mph |
| Design Details | |
| On Street Parking | Yes |
| Junction Radius | 8m |
| Traffic Calming | Chicanes/limited sightlines/ raised thresholds |
| Plot Boundary Treatment | Hedgerow/Shrubs/Trees/ brick and stone walls |
| Materials | |
| Footway Surfacing | Black Tarmac |
| Carriageway | Black Tarmac |

Streets - Key Design Principles

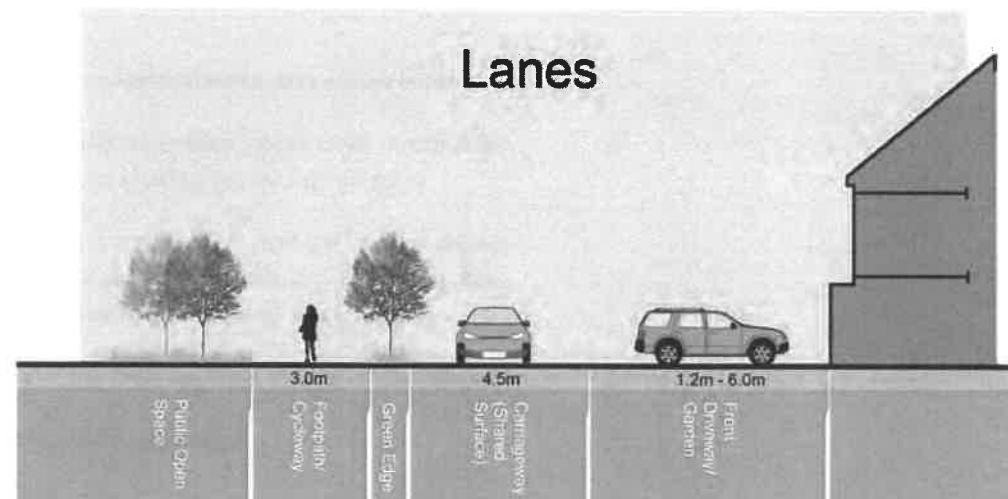
- Secondary routes in the hierarchy accommodating medium traffic flows
- Clear definition between carriageway and footpath
- Mix of house types including detached, semis and townhouses
- Informal character with variety of boundary treatments and building setbacks
- Houses have direct vehicular access onto the street
- Block paving introduced where routes cross central green corridor
- Footway along one side only where route adjoins public open space



| Criteria | Street Type: Lanes |
|-----------------------------------|---|
| Dimensions & Character | |
| Carriageway Width | 4.5m |
| Footway | shared surface |
| Service Strip | Within carriageway |
| Direct Access to plots | Yes |
| Building Setback | Varies (typically 1.2-6m) |
| Design Speed | |
| Target Speed | 20 |
| Design Details | |
| On Street Parking | Yes |
| Junction Radius | 6m |
| Traffic Calming | Carriageway width, boundary planting, surfacing |
| Plot Boundary Treatment | Hedgerow/Shrubs/Trees |
| Materials | |
| Carriageway | Block Paving (Red) |

Lanes - Design Principles:

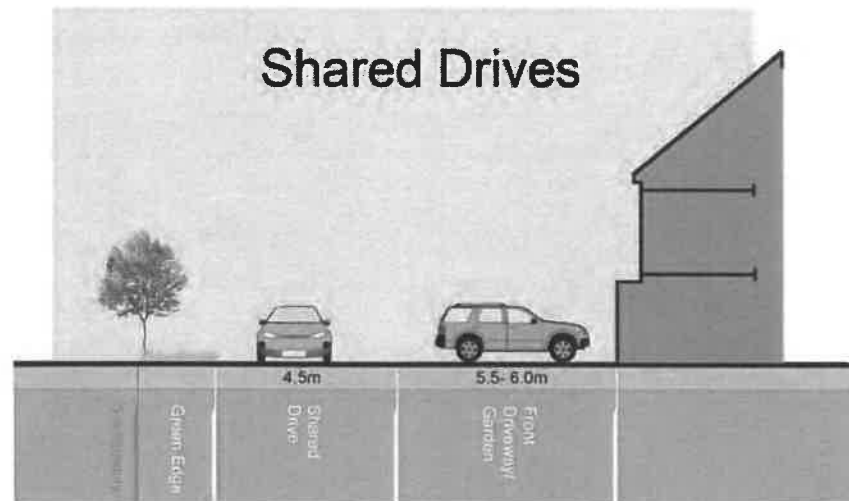
- Tertiary route in the hierarchy accommodating lower traffic flows and serving small numbers of properties fronting onto central green corridor
- Provide link between adjoining streets
- Shared surface route with vehicles and pedestrians sharing the same space
- Informal character - adjoining public open space with tree planting along verges
- Mix of house types including detached, semis and townhouses
- Houses set back from the road boundary behind frontage parking
- Houses have direct vehicular access onto the street



| Criteria | Street Type: Private Drives |
|-----------------------------------|---------------------------------|
| Dimensions & Character | |
| Carrageway Width | 4.5 |
| Footway | n/a |
| Service Strip | 1m grass verge |
| Direct Access to plots | Yes |
| Building Setback | Varies (typically 5.5-6m) |
| Design Speed | |
| Target Speed | 10mph |
| Design Details | |
| On Street Parking | Yes |
| Junction Radius | 6m |
| Traffic Calming | Shared space, restricted length |
| Plot Boundary Treatment | Hedgerow/Shrubs/Trees |
| Materials | |
| Surfacing | Block Paving |

Private Drives - Key Design Principles

- Shared private drives serving maximum of 4 houses
- Serve houses around the edges of the site
- Typically single sided with houses fronting open space or green edges
- Houses have direct vehicular access onto the street
- Localised widening of carriageway to provide on-street visitor parking
- Shared surface space
- Informal street character
- Block paving to carriageway softens transition with adjacent open spaces
- Houses set back from the road boundary behind frontage parking





Hard Landscape Material Palette

Figure 05:06 opposite illustrates the proposed hard landscape materials palette.

High quality, durable materials will be used throughout the development.

The hard landscape elements have been drawn from a simple palette to reflect those found in the surrounding area and to complement/reinforce the materials within Phase 1.

The materials palette will be used to identify and reinforce the proposed movement hierarchy. Private drives for example will be finished in red block paving whereas the main spine road will be traditional in form with tarmac carriageway and pavements. Shared-surface lanes will be finished in either tarmac or red block pavers.

In summary, the hard landscape materials palette comprises:

- black tarmac carriageway and pavements;
- concrete kerb edges;
- grey concrete paving flags to private plot paths;
- Marshall Herringbone Block Pavers in Red to drives and traffic calming points; and
- Coloured tarmac cycleway/footpaths (buff).

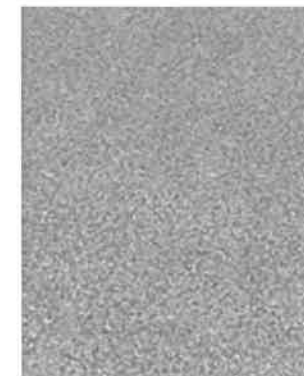
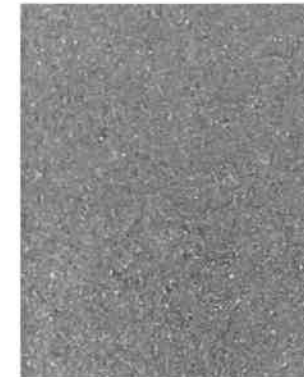




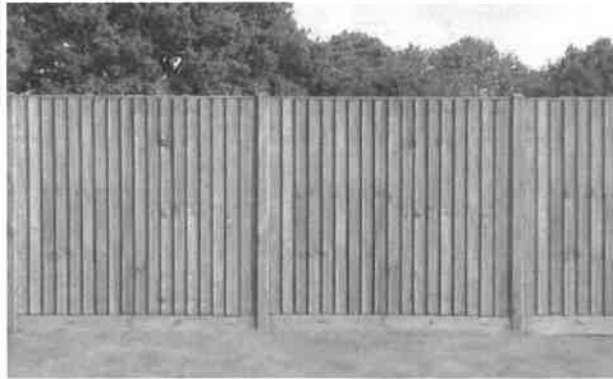
Figure 05:06: Hard Landscaping Layout Plan

Boundary Treatments

The site incorporates a variety of robust, attractive fence types and boundary treatments to clearly delineate public and private spaces, identify linkages, and guide residents and visitors through the scheme. The specification of boundary treatments with example images are shown on this page.

Boundary Types:

- 1800m Closeboard Fence
- 1800m Brick Boundary Wall
- 1800m Stone Boundary Wall
- 1200m Ball Top Railings
- 1200m Hedge



A. 1800MM HIGH BOUNDARY WALL WITH TIMBER FENCE; 550MM WALL

D. 1800MM HIGH BOUNDARY WALL WITH TIMBER FENCE; 600MM WALL (Reconstituted Stone Option)

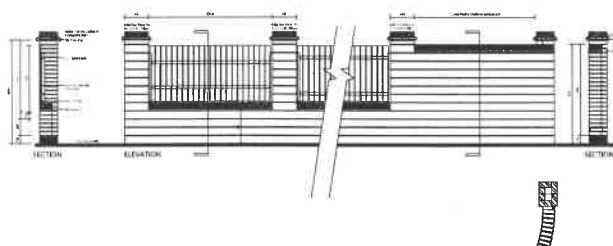
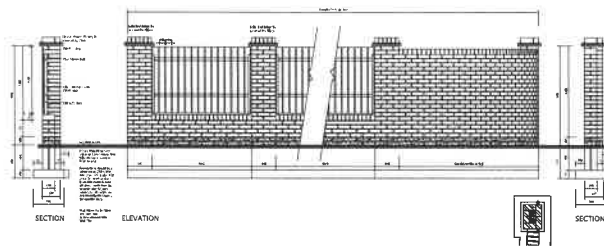


Figure 05.07: Boundary types (for illustrative purposes only)

Local Vernacular

The images opposite illustrate some of the typical architectural styles, house types, detailing and materials found on residential properties within the vicinity of the site. This analysis has informed the detailed design of the proposed house types which incorporate a number of architectural elements found locally.

Some typical characteristics include:

- Housing typologies include detached and semi-detached houses, townhouses and bungalows.
- Most domestic properties are two storeys in height.
- Brick is the predominant construction material within the immediate context of the site and is utilised for building and landscape uses, such as garden walls. Some variation exists in colour, however the general palette is light brown and red. Stone is also common on many of the older terraced houses.
- Simple architectural detailing using features such as porches, bay windows, corbals, contrasting materials, window and door surrounds and gables to provide interest to building facades.
- Primarily 2 storey detached houses along Redwood Drive. Predominant materials include light brown and red brick, render, red and grey roof tiles. Typical detailing includes mock tudor timber detailing to first floors, contrasting brick courses, artstone cills and lintels and decorative bargeboards to front gables.
- Dormer bungalows along Thornfield Avenue with rounded bay windows and light brown brick to exterior walls.
- Older terraced properties along Inglewhite Road, Crumpax Avenue and Davis Street. Materials include a mixture of brick and stone to exterior walls and grey slate roofs. Arched stone door frames with fanlights are a distinctive feature of some terraces along Inglewhite Road.
- Distinctive semi-detached houses along Inglewhite Road to east of Chipping Lane with double height rectangular bay windows and decorative finials to front gables.
- Small development of bungalows along Crumpax Meadows.



Figure 05:08: Local Vernacular Housing

Appearance & Detailing

The elevation designs have been informed by our vision to create a unique, high quality development that respects the character of the local area.

The design intent has been to develop an attractive traditional style with subtle variation introduced in the architectural treatment of houses to help create areas of distinct character within the development.

The proposed development respects the local vernacular and utilises various design features to create interest in the appearance of the development as a whole

Design cues have been taken from our own observations and analysis of the local vernacular (see above).

Figure 05:09 opposite shows some of the elevation features and detailing on the proposed house types.

Figure 05.10 illustrates a range of Barratt standard house types, including several Phase 1 sales. These are shown for illustrative purposes only but give a flavour of the design intent for this site.

Key Elevation Features:

- Contrasting smooth blue brick course to front elevations;
- A strong sense of symmetry, rhythm and balance in the treatment of main facades and fenestration;
- Decorative gable vents;
- Variety to the roofscape provided through the use of traditional pitched roofs, dormer windows, front gables, chimneys and variation in ridge height;
- Artsone heads and cills;
- Combination of different materials of principal facades to create visual interest;
- Glazed panes to garage doors;
- Front entrances articulated with porches and canopies of various designs; and
- Consistent use of proportions and details throughout site to create a coherent rhythm.

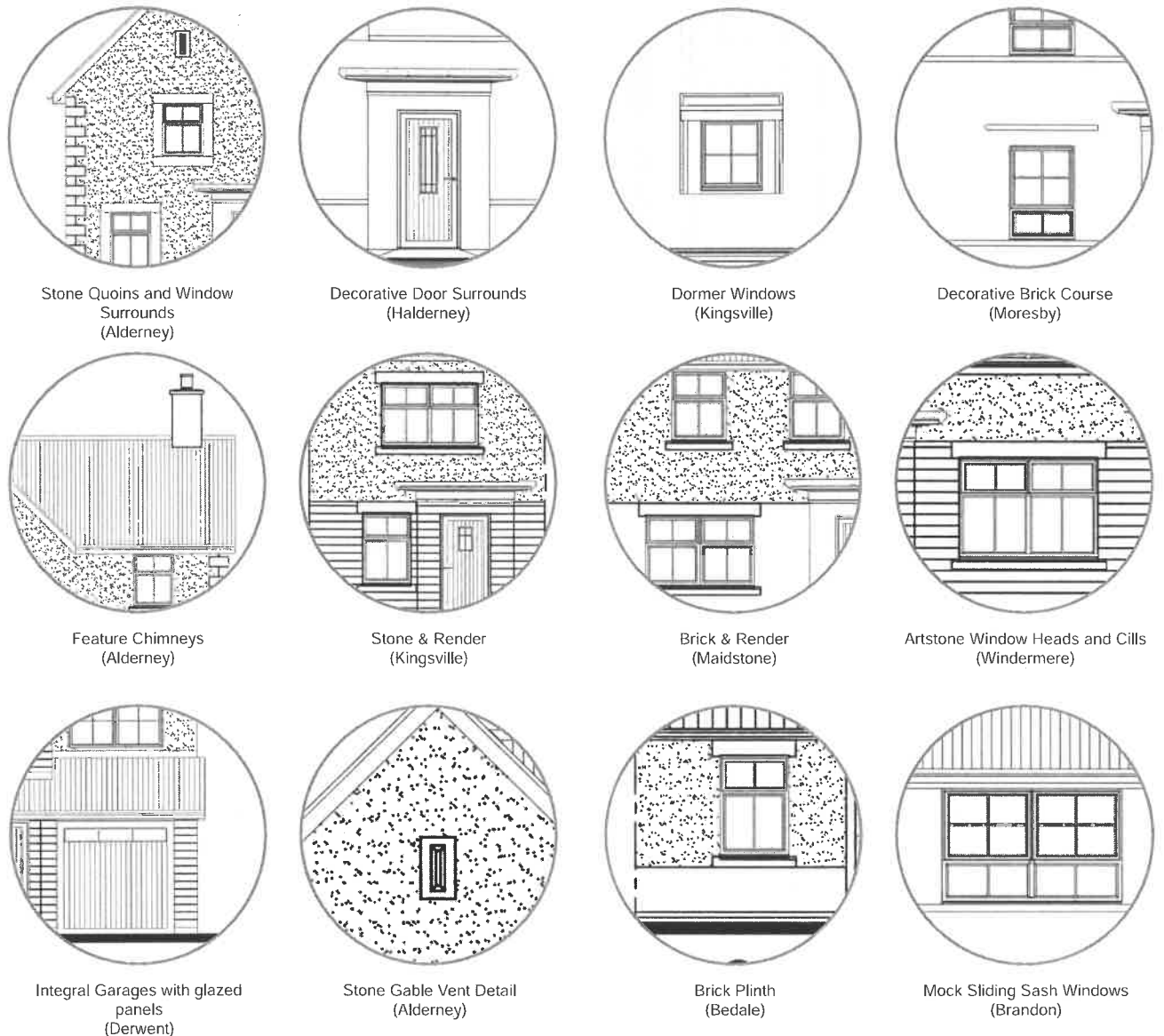


Figure 05.09: Typical detailing



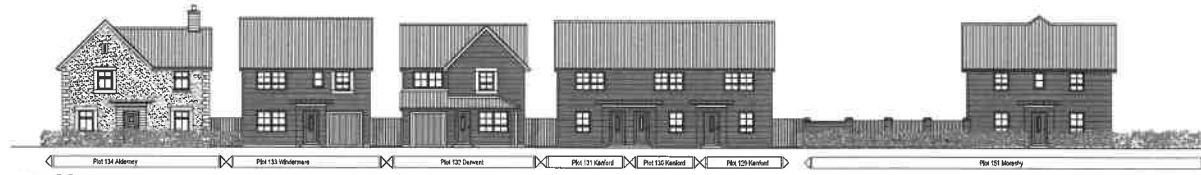
Figure 05.10: House Type CGIs (for illustrative purposes only)



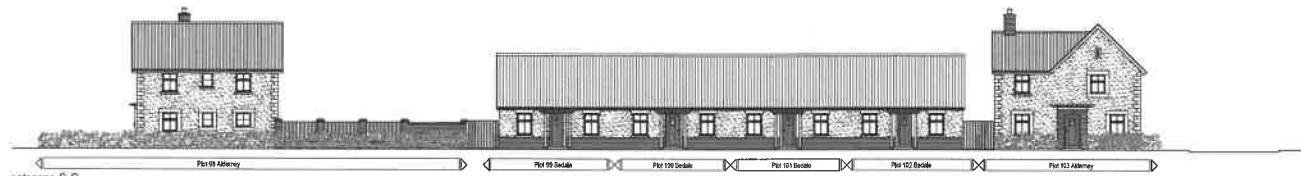
setscape A-A



setscape A-A Continued...



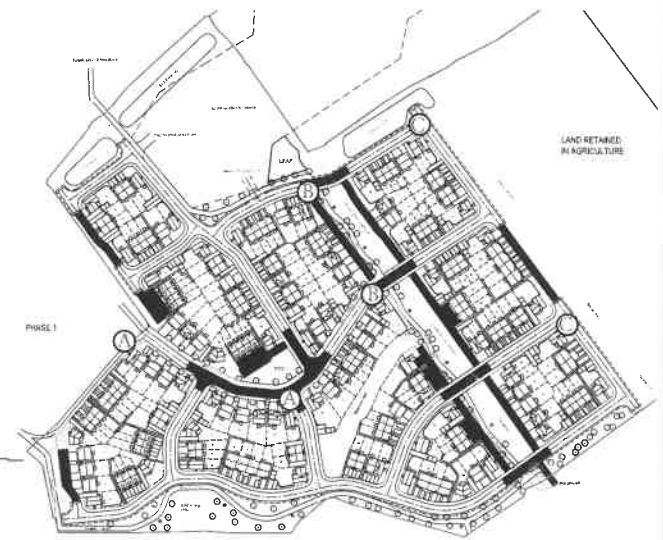
setscape B-B



setscape C-C



setscape C-C Continued



Site Layout (NTS)

WARNING TO VISUAL PROMINENCE
 Property Information Act 1991
 This plan is a simplified representation of the proposed development. It is not intended to be used as a basis for any legal proceedings. The design and appearance of the proposed development is subject to change without notice. The design and appearance of the proposed development is subject to change without notice. The design and appearance of the proposed development is subject to change without notice.



Figure 05.11: Streetscenes



Building Materials

The proposed building material palette has been carefully selected to respond to the local vernacular. Brick type and colour has been chosen to complement traditional building materials found locally. Subtle variation in brick type, introduction of render, brick detailing and different colour garages and front doors will be used to help create variety and character within the development.

On most units, two or more different materials have been used in combination to provide visual interest and variety. For example, a number of units use contrasting brickwork bands or selective use of render on upper levels.

The materials plan opposite (Figure 05:13) illustrates how different materials have been used and combined to create character areas within the development. For example, reconstituted stone is used on principally houses along the main spine road whereas Forterra Woodside mixture brick type is used primarily on houses along the eastern boundary and interface with the retained agricultural land.

Key Material Specification:

Facing Bricks

- Forterra Woodside Mixture
- Arden Special Reserve
- Forticrete Recon Stone (Black Old Weathered - split finish)

Detail Brick

- Smooth Blue

Render

- Monocouche Ivory Render

Roof Material

- Russell Grampian (grey)

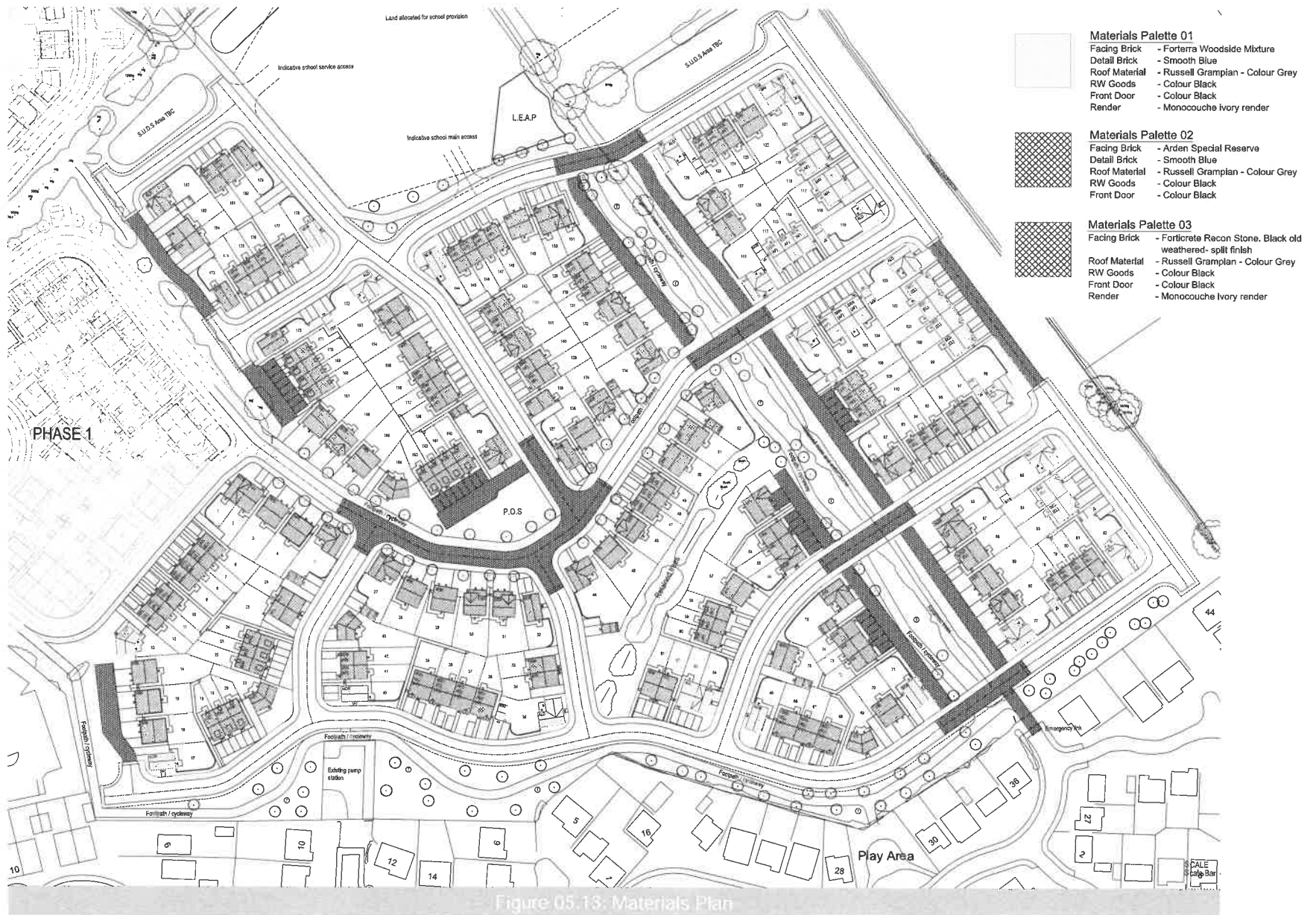
Rainwater Goods

- Black

Other: Artstone heads and cills



Figure 05.12: Building Material Palette



Landscape

Figure 06.01 opposite illustrates the proposed landscaping strategy for the site.

The development of the masterplan for this site has taken a strong landscape-led approach, culminating in a high quality development, set within a mature landscape structure. A high proportion of the site has been set aside as green infrastructure, creating a sense of openness within the development and providing a strong green character appropriate to the site's setting.

Key elements of the landscape strategy for the site include:

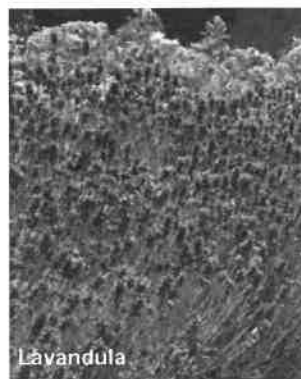
- Areas of SuDS and attenuation ponds will provide opportunities for increased habitats and ecological benefits;
- Evergreen shrub planting to property frontages to provide a sense of enclosure and privacy as well as year round interest and colour;
- An avenue of trees extending from the site access into the main part of the site, placed at regular intervals along the central spine road to create an attractive street scene and provide internal greenery and seasonal interest;
- Retention of the existing belt of planting through the centre of the site that will be enhanced with additional native shrub and tree planting;
- Enhancement of existing native hedgerows to boundaries and additional areas of native hedgerow planting;
- A landscape buffer of between 12 and 22 metres between the development and the cricket pitch and primary school site that will include native hedgerow planting and areas of scattered native tree planting to filter views. An area of Sustainable Urban Drainage located on the north east corner of the site will further soften this edge and include areas of native marginal planting at its extents to provide additional ecological benefits;
- A generous landscape buffer (12-13.5m) located along the eastern boundary and interface between the development and retained agricultural land that allow for the retention/enhancement of existing native hedgerow and tree planting with additional tree planting to filter views from the AONB/Forest of Bowland;

- The provision of an internal green corridor (16-18m wide) through the site following the line of the existing watercourse, incorporating new areas of native tree planting, a pedestrian footpath link and trim trail equipment;
- A native hedgerow will be planted to the southern boundary of the site along the interface between new and existing houses and will include scattered trees, an understorey wildflower meadow planting, footpath links and trim trail equipment. The buffer width will vary from from 8-30 metres;
- A central area of public open space providing a focal point within the development along the main Avenue and benefitting from good surveillance from surrounding houses;
- Wildflower meadow will help to integrate the development with adjacent planting and provide ecological and amenity benefits; and
- Evergreen hedgerow planting to property boundaries will soften the internal scheme and provide visual amenity through the whole year and plot privacy.





Figure 06.01: Landscape Plan



Trees

Shrub & Hedgerow Planting Mix

Figure 06.02: Typical Species

Sustainability

Barratt Homes are committed to sustainable development. These development proposals have considered the existing site context and employ best practices to minimise the environmental impact of both the construction of the scheme and its future use as a residential environment.

Existing trees and hedges will be retained and enhanced as required in accordance with the landscape proposals. The existing topography and site levels have been considered in order to minimise earthwork arising, reducing the volume of waste disposed to landfill.

Advice within the supporting reports accompanying this application demonstrate that the site is within a sustainable location, and as such residential development is therefore acceptable and in accordance with national and local policy. The site provides excellent transport linkages and is well served by public transport including regular bus services with a bus stop nearby, and the local road network is generally suitable for cycling.

In line with current best practice, surface water run-off will be managed in a sustainable manner, with surface water attenuated to prevent surface run-off in excess of current greenfield rates. This is addressed in the separate Drainage Strategy.

Specific measures employed by Barratt Homes to promote sustainability include:

- The provision of cycle storage to all dwellings to reduce the number of journeys by car.
- Use of A Rated appliances which are energy efficient and help reduce CO2 emissions from the dwelling.
- Provision of SEDBUK A Rated boilers which are more energy efficient.
- Provision of an internal / external space in each dwelling for drying clothes – thus minimising energy use.
- Use of materials from sustainable sources e.g.: Timber from renewable forests.
- A Site Waste Management Plan is to be prepared for the development as part of our Construction Phase Safety, Health and Environmental Plan. Currently over 80% of construction

waste from our sites is sorted at source and recycled.

All our suppliers are required to use materials from sustainable sources or be ISO14001 accredited themselves. All timber is used in our developments is sourced sustainably.

Barratt undertake a number of measures to reduce the amount of potable water used on our developments, with examples including the use of flow restrictors on its taps and showers and dual-flush toilets to reduce water usage to 125L/person/day.





Summary

This Design Statement supports a reserved matters application for Phases 2 & 3 of the residential development of land east of Chipping Lane, Longridge. Outline planning permission for the development of up to 363 dwellings was secured in 2015. This reserved matters application seeks approval for the appearance, landscaping, layout and scale of the development, of Phases 2 & 3 of the development comprising 184 dwellings.

The subject site extends to 10.56ha and is located on the northern edge of the town of Longridge within the administrative area of Ribbles Valley Borough Council. The site is currently existing farmland used for pasture. The site comprises a number of field parcels, with mature hedgerow to the north, west and east.

This design statement explains how the current proposals have emerged from analysis of the site and its context, outlining the design process followed and the technical considerations that have informed the final layout. This document has demonstrated how the development has taken into account the masterplan parameters established in the outline planning application. It also sets out the design principles underpinning the current proposals in relation to matters such as site layout, movement, scale, density, house design and landscaping.

The proposed development of 184 new homes will make an important contribution to meeting housing need in the local area, including the provision of much needed affordable homes. This will be a high quality development that respects the local character and wider landscape setting.

In summary, the development:

- Provides a range of housing types and sizes to appeal to a broad market, providing housing choice and diversity locally;
- Is consistent with the design principles and parameters set at the outline planning stage;
- Sits comfortably alongside existing residential development in the immediately surrounding area being of an appropriate character, scale, density and form;
- Provides a legible and efficient arrangement of streets, lanes and footpaths that encourages non-motorised modes of transport for short journeys;

- Provides a high amenity residential environment for enjoyment by existing and future residents with areas of open space and high quality hard and soft landscaping;
- Integrates with, retains and enhances existing green infrastructure including existing hedgerows and mature trees;
- Offers the opportunity to finish the edge of the settlement with outwards facing properties and a strong green infrastructure edge to the town.
- Respects the constraints and opportunities of the development site, including preserving the amenity and privacy of existing houses on adjoining sites and retaining existing natural assets; and
- Provides a strong green infrastructure network with space for natural habitats and movement corridors for wildlife.

Conclusions

Overall, it has been demonstrated that the proposed development is of a high quality design that has regard to, and is consistent with, the parameters set by the outline approval. We therefore request that planning permission is granted for this scheme.

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