



DESIGN & ACCESS STATEMENT

Proposed Residential Development at Pendle Road, Clitheroe.

Phases 2, 3 & 4

September 2019

CONTENTS

SECTION 1 INTRODUCTION

- INTRODUCTION

SECTION 2 CONTEXT ANALYSIS

- REGIONAL & DISTRICT SETTING
- CONTEXTUAL ENVIRONMENT - VISUAL ANALYSIS
- NEIGHBOURHOOD & LOCAL SETTING
- HISTORIC EVOLUTION

SECTION 3 SITE ANALYSIS

- CONSTRAINTS & OPPORTUNITIES

SECTION 4 DESIGN RESPONSES

- SCHEME EVOLUTION
- BUILDING FOR LIFE

SECTION 5 THE SCHEME

- USE & AMOUNT
- LAYOUT & SCALE
- CHARACTER AREAS
- VISUAL LINKS
- MOVEMENT AND CONNECTIONS
- LANDSCAPING
- APPEARANCE

SECTION 6 SUMMARY

- SUMMARY

1.1 This Design and Access Statement (DAS) has been prepared by Baldwin Design Consultancy Limited on behalf of Taylor Wimpey Manchester in support of their reserved matters planning application for residential development comprising of 426 dwellings in Phases 2, 3 & 4 at the site located off Pendle Road, Clitheroe.

1.2 This application seeks planning permission for the development of greenfield agricultural land for residential purposes. This planning application constitutes the second reserved matters planning application submitted pursuant to outline planning consent 3/2012/0942 and updated outline planning consent 3/2015/0895.

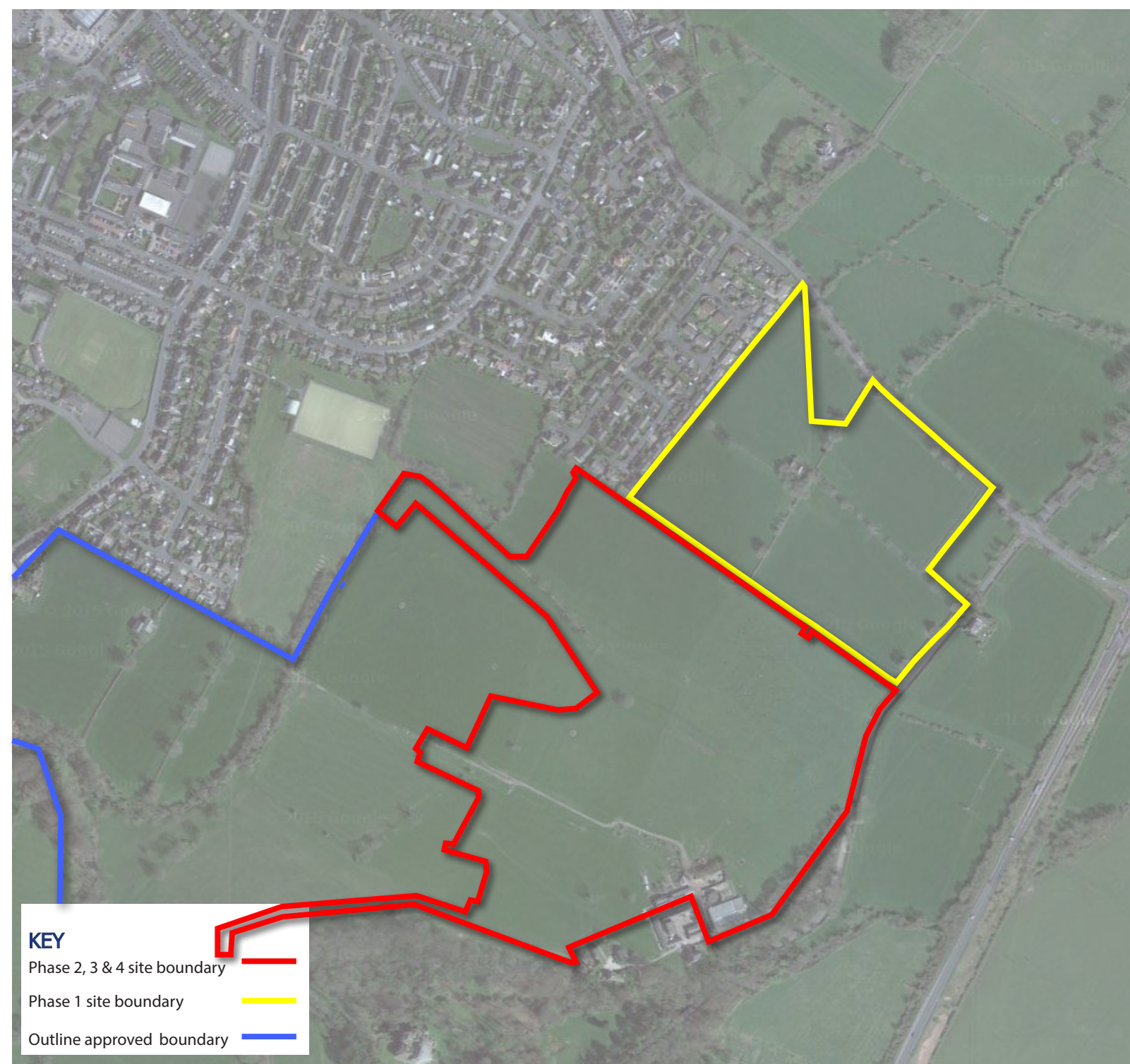
1.3 This document should be read in conjunction with the supporting planning statement submitted as part of this reserved matters planning application.

1.4 The purpose of this report is to describe the site, its context, relevant design guidance and the design concepts and principles which will inform the final design.

1.5 The structure of this document and the detail within follows the guidance from CABI's 'Design and Access Statements: How to read and use them' (2006) and Part 2, Article 8 of the Town and Country Planning (Development Management Procedure)(England) Order 2010.

1.6 This Statement is set out as follows:

- **Section 2:** Context Analysis - an assessment of the site's wider context in terms of town structure, historic development, streets and landscape character.
- **Section 3:** Site Analysis - a more detailed focus of the site's constraints and opportunities to be read in conjunction with the site analysis previously described within the Outline approved Design and Access Statement.
- **Section 4:** Design Responses - key design principles which have informed the layout design, along with responses from the public consultation.
- **Section 5:** The Scheme - parameters for the site, supported by a final layout that demonstrates how the site can be developed in accordance with the parameters including key design principles which shows how the scheme ties into the surrounding vernacular, incorporating architectural details, materials, and scale of building, with reference to landscaping.
- **Section 6:** Summary



Aerial view of proposed development

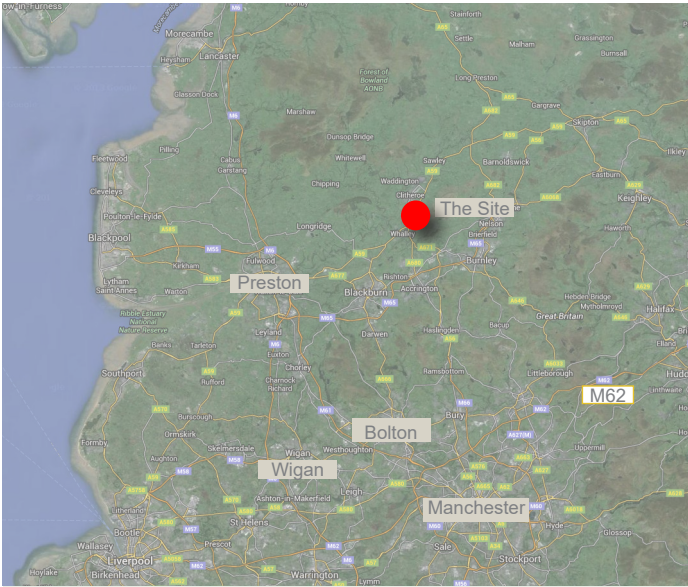
Context Analysis

Regional & District Setting

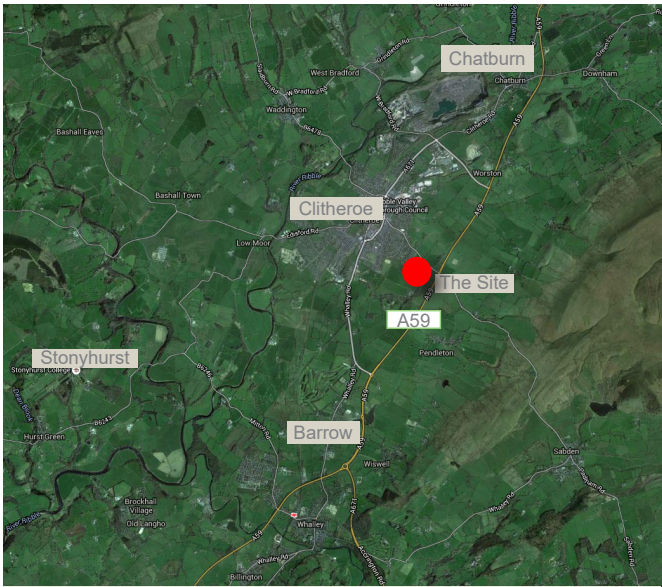
2.1 Clitheroe is a town and civil parish in the Borough of Ribble Valley in Lancashire, England. It is near the Forest of Bowland and is often used as a base for tourists in the area. The most notable building in the town is Clitheroe Castle, suggested to be one of the smallest Norman keeps in the country. Clitheroe is approximately 55 miles (34 KM) North-North West of Manchester.

2.2 Train services - There are hourly trains to Blackburn and Manchester Victoria from the railway station that are operated by Northern Rail. Usually, services are operated by Class 150 trains, but sometimes Class 156 and Class 153 operate the service. The Ribble Valley Rail group (community rail group) is campaigning for services from Clitheroe to be extended to Hellifield.

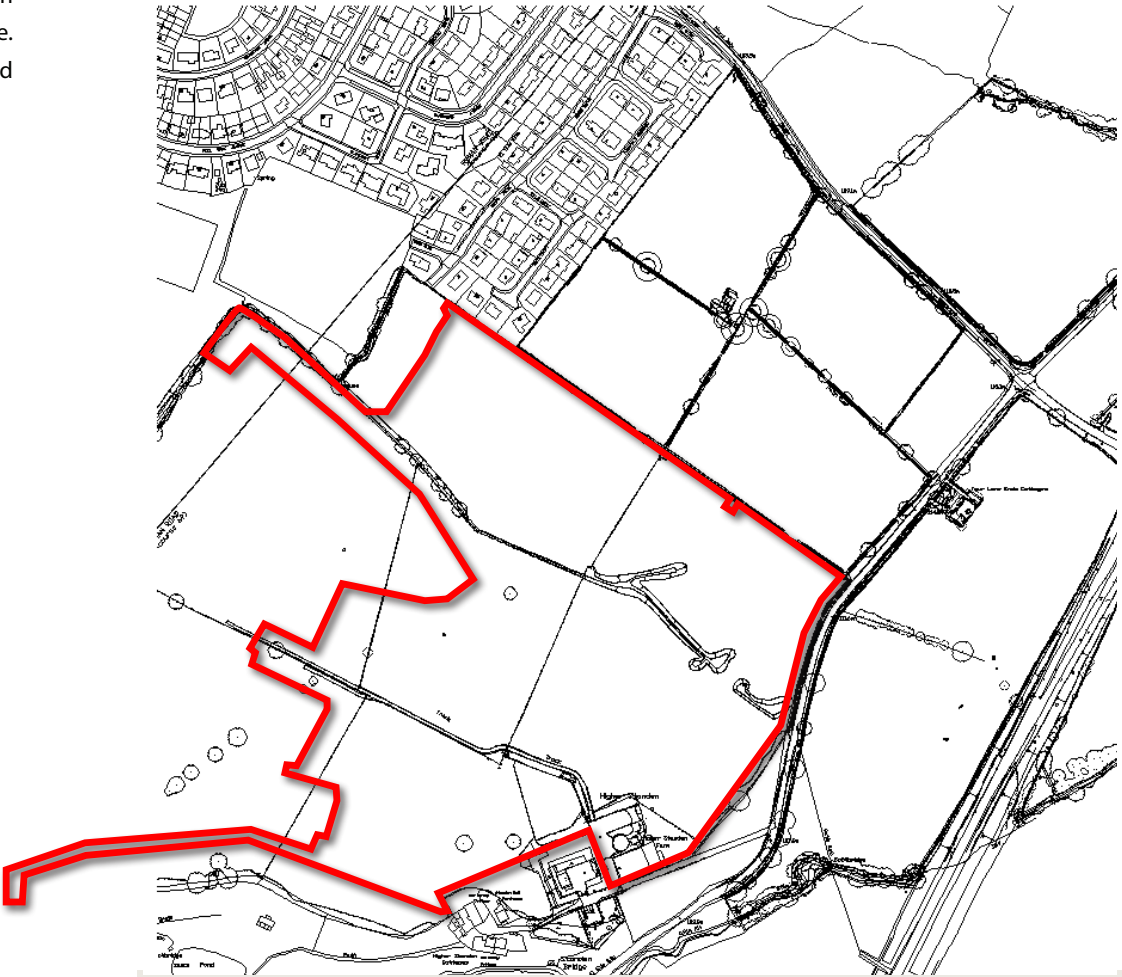
2.3 Bus services - There are frequent bus services from Clitheroe Interchange to the surrounding Lancashire and Yorkshire settlements. Transdev is the most prominent operator, mainly operating interurban services to other towns in Lancashire, Greater Manchester and Yorkshire. Other operators include Holmeswood Coaches, M&M Coaches, Little Red Bus & Tyrer Bus.



Regional Context Setting



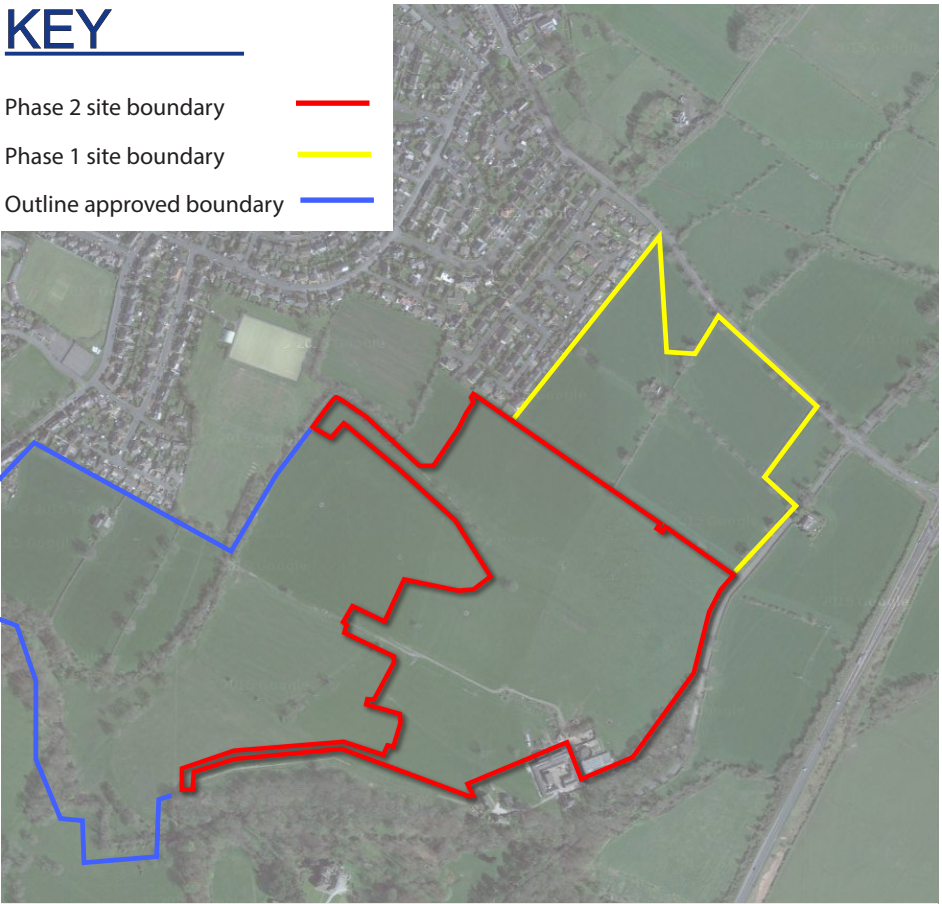
District Context Setting



Site Location Phase 2

KEY

- Phase 2 site boundary
- Phase 1 site boundary
- Outline approved boundary



Aerial View of Site Phase 2

Visual Analysis



View 1 Pendle Road looking towards north west



View 2 looking south west down Worston Road



View 3 looking south east down Pendle Road showing existing dwellings to the right



View 4 from Pendle Road looking towards site entrance and new roundabout



View 5 looking towards the cross roads with Pendle Road to the left hand side



Locator

Context Analysis

Neighbourhood Setting

2.5 The Ordnance Survey map on the right shows the Phase 2, 3 & 4 development site edged in red and local built environment blocked out in grey.

2.6 Clitheroe is built on the north west of the A59 which is the main route from Blackburn to Skipton. Off the A59 you can access the M65 where you can access the main towns of Preston to Burnley.

2.7 The existing buildings within Clitheroe are varied in styles, age and materials. The town hosts a number of historic buildings with architecturally attractive features including thatched roofs, stone window and door surrounds and chimney stacks etc. The dominant buildings materials are brick and render.

2.8 New housing estates have expanded the area and these have been predominately built with red and buff brick and render, with a varied mixture of built forms. Typically in detached and semi detached fronting the road.

2.9 Reasonably sized front gardens provide adequate space for off road car parking.

2.10 The surrounding housing stock to the development site are generally 1 and 2 storey brick and rendered buildings with pitched grey and red tiled roofs.

Local Setting

2.11 Neighbouring residential developments off Pendle Road are characterised mainly by two storey red/brown brick and rendered residential properties, with a varied mixture of built forms, typically in mews, detached and semi detached fronting the road.

2.12 Reasonably sized front gardens provide adequate space for road car parking where plot boundaries to the back of the existing footpath are delineated with a varied mix of boundary treatments in the form of hedge planting and low level walling/fence.

2.13 There are a number of local amenities within close proximity of the development in Clitheroe Town Centre, including food and drink establishments, shops, pharmacy and Post Office etc. The site also has links to existing open space surrounding the boundaries of the development.

2.14 The site benefits from good accessibility with a number of transport hubs in close proximity. This gives the site great accessibility to the local, regional and national road networks, including bus routes, and thus provides opportunities for modes of transport other than the car.



View from the top of Clitheroe Castle



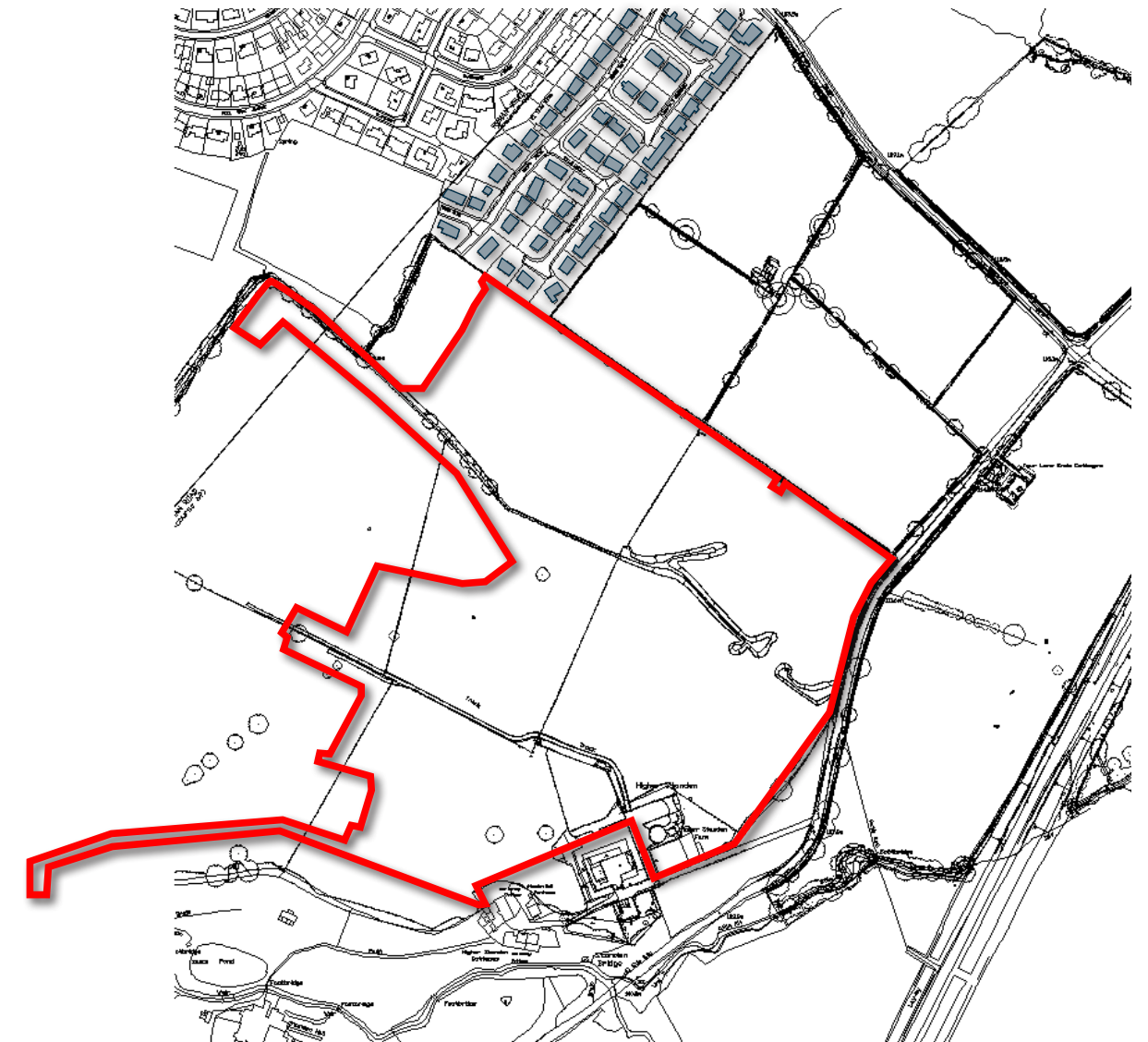
View of Clitheroe Castle



Existing development off Shays Drive



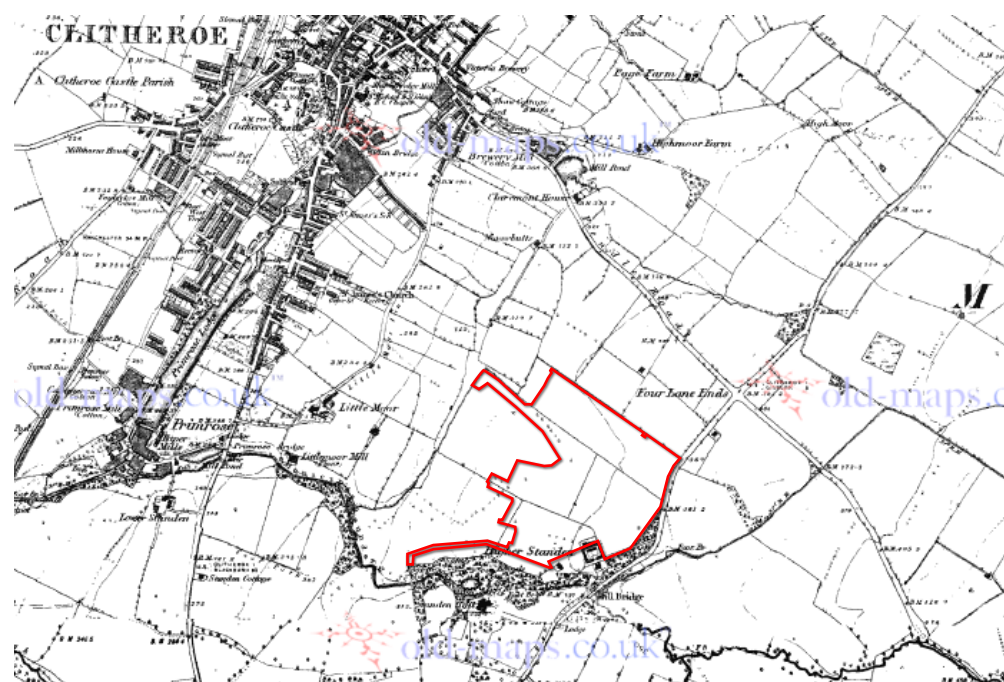
Typical street in Clitheroe



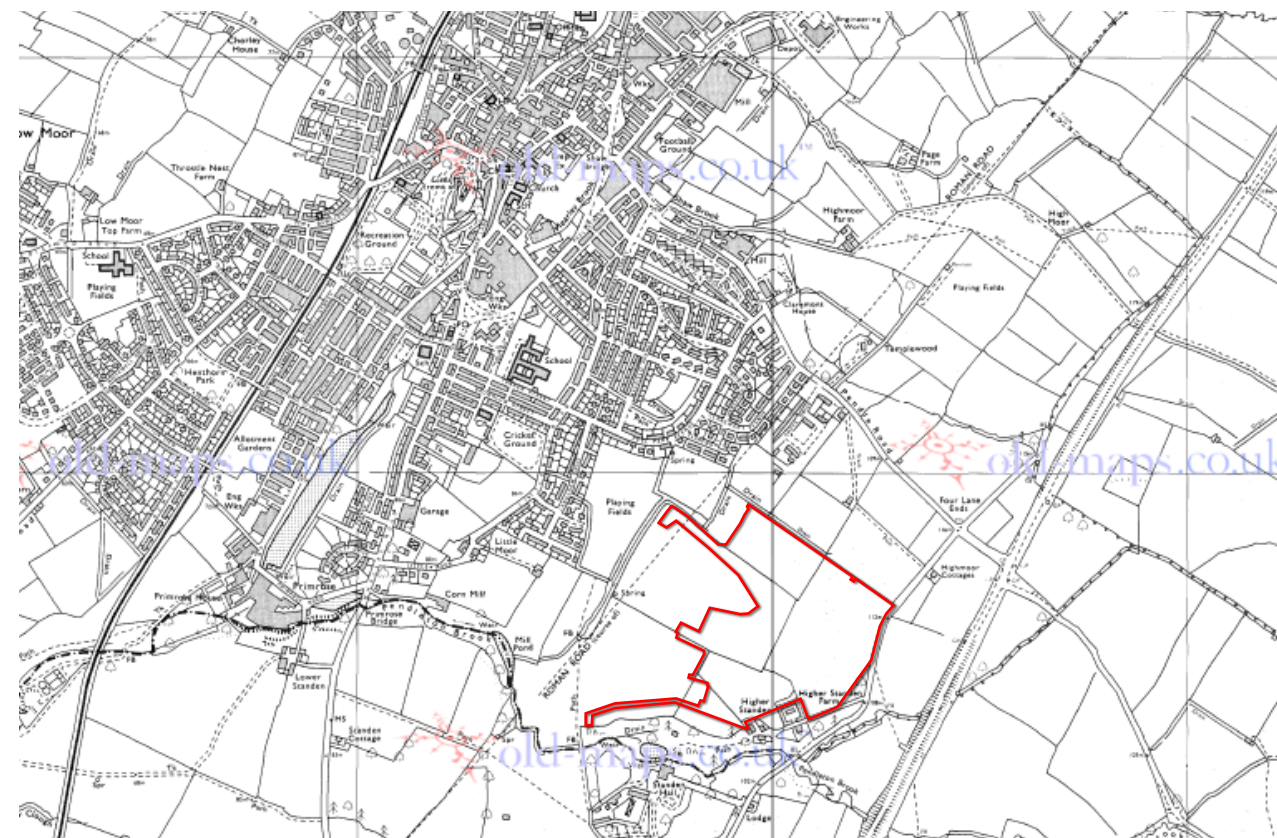
Site Location Phase 2

Historic Evolution

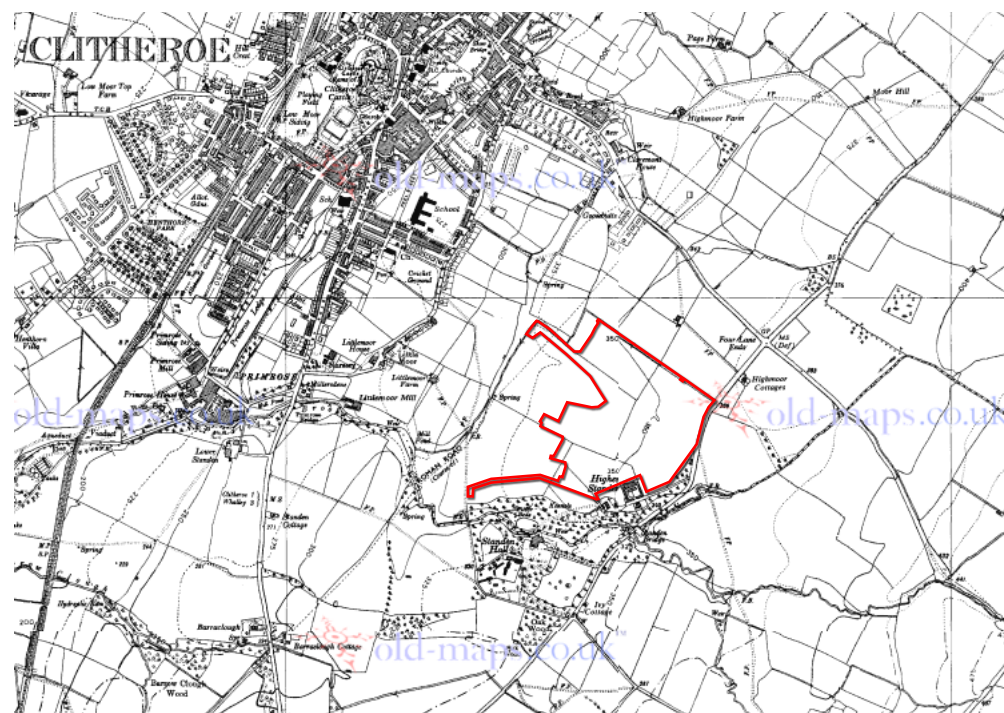
2.15 The evolution and growth of the area has expanded in alignment with population growth. The following images are a demonstration of how the local area of Clitheroe has expanded and become the settlement we see today.



Historic Urban Grain taken in 1895



Historic Urban Grain taken in 1975



Historic Urban Grain taken between 1955-1956



Current aerial view of development site Phase 2, 3 & 4

Site Analysis

Constraints and Opportunities

3.1 Key opportunities and constraints for the development of the site have been identified following a detailed appraisal of the site and its context. The points are summarised on the following plan.

KEY

Site Boundary Phase 2, 3, 4

Site Boundary Phase 1

Existing low rise residential properties

Medium range views into site

Long range views into site

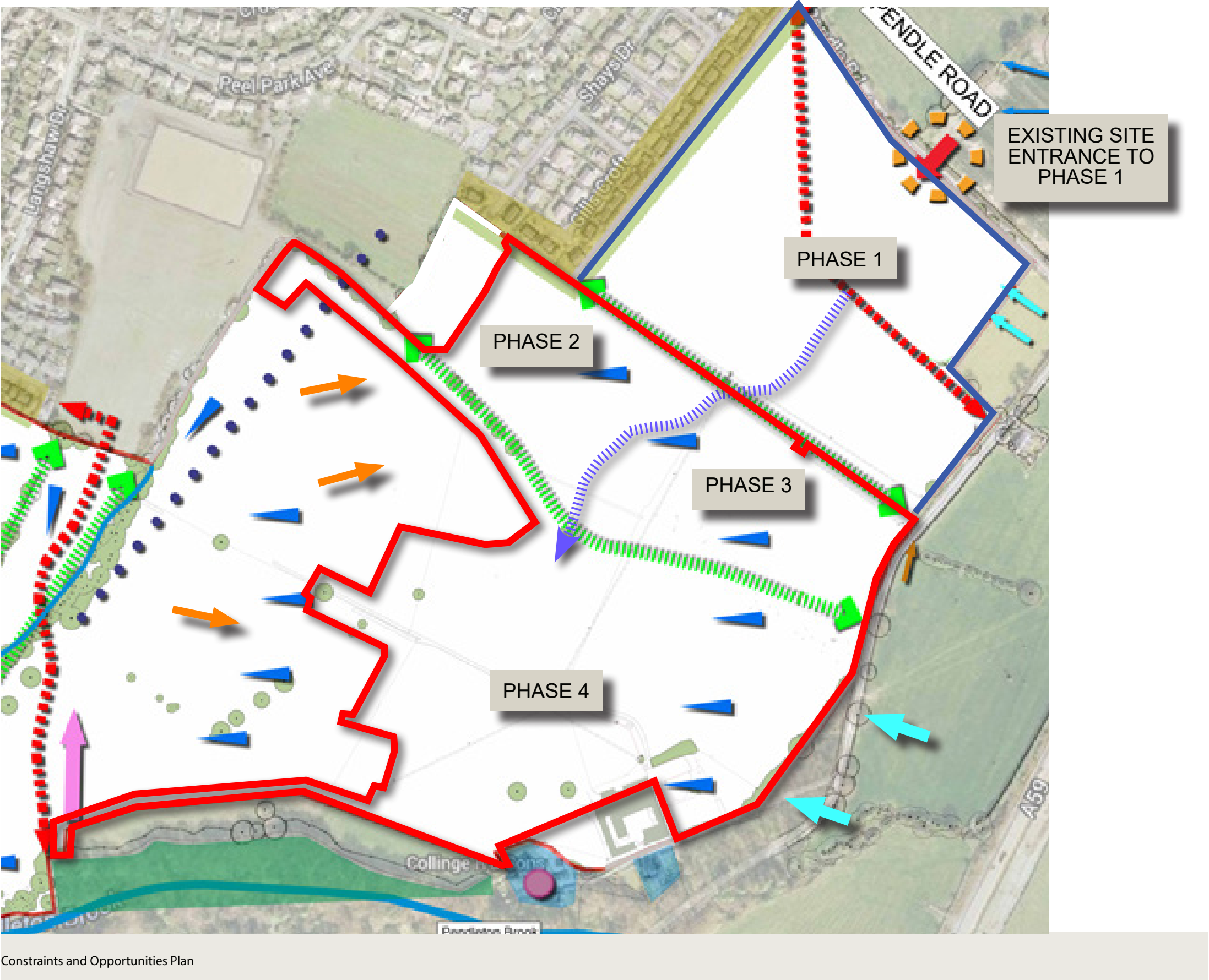
Public Right of Way in Phase 1

Site slopes

Main site access from A59 to Phase 2, 3 & 4 from Phase 1

Opportunities for Green Corridors

Main spine road to access phase 2, 3 & 4 from existing phase 1



Constraints and Opportunities Plan

Scheme Evolution

4.1 During the course of producing the Masterplan, options were considered to ensure the correct form of development be achieved in a final proposal. This process has resulted in the optimum form of development most appropriate for the area and responding to site constraints. Demonstrated below are illustrations of how the scheme has developed from the earlier stages to planning submission.



Concept Scheme

- Site access to Phase 2 and 3 to be gained off the main Spine Road to Phase 1
- Development served off private drives to front areas of green open space
- Proposed properties to be backed onto existing bungalows on Gills Croft
- Retention of existing trees and hedges on the edges of the development
- Building footprints set outside tree protection areas
- Focal points set in key locations throughout the scheme
- Massing and height of built form increased
- A network of internal roads creating good connectivity
- 1 block of apartments introduced



Scheme 1

- Site access to Phase 2 and 3 be gained off the main Spine Road to Phase 1
- Additional 51 plots added to Phase 3
- Development served off private drives to front areas of green open space
- Junctions off the main spine road staggered
- Retention of existing trees and hedges on the edges of the development
- Bungalow house types added to side up to Gills Croft rather than backing on
- Building footprint set outside existing drainage easement
- Single sided road to front east parcel where phase 3 meets existing phase 1
- Apartments introduced with rear parking
- A network of internal roads creating good connectivity

Design Responses



Scheme 2

- Build line along east boundary (Worston Road) stepped forward to give more interest to the street scene
- Additional 205 plots to Phase 2, 3 and 4
- Tree lined avenue applied to the main spine road
- Single sided road omitted from east parcel
- Amended sales area to each parcel
- Retention of existing trees and hedges
- Single sided road added to west parcel and the bungalows to front the existing bungalows on Gills Croft
- A network of internal roads creating good connectivity
- Affordable units re-jigged

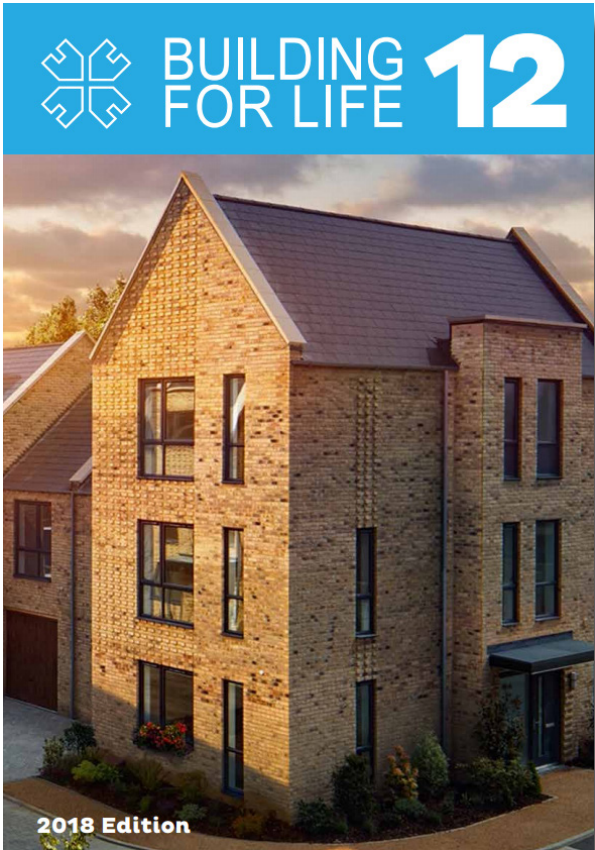


Planning Application submitted scheme

- Layout produced in Autocad format as a final scheme
- 2.5 storey height buildings added to key locations
- Single sided road omitted from east parcel
- Build line along east boundary (Worston Road) stepped forward to give more interest to the street scene
- Tree lined avenue applied to the main spine road
- Amended sales area to each parcel
- Layout form pretty much replicates scheme 2, 3 & 4 sketch with just an introduction of new house types
- Affordable units re-jigged
- Single sided road added to west parcel and the bungalows to front the existing bungalows on Gills Croft
- A network of internal roads creating good connectivity
- Apartments omitted
- Attenuation pond added



- Layout produced in Autocad format as a final scheme
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4.2 A 'Building for Life' assessment has been produced by Baldwin Design Consultancy Ltd in accordance with the 'Building for Life 12' questions.

Building for Life Question

Response Grade :

Stop consider good

1. Does the scheme integrate into its surroundings by reinforcing existing connections and creating new ones; whilst also respecting existing buildings and land uses along the boundaries of the development site?

1a Where should vehicles come in and out of the development?

1b Should there be pedestrian and cycle only routes into and through the development? If so where should they go?

1c Where should new streets be placed and could they be used to cross the development site and help create linkages across the scheme and into the existing neighbourhood?

1d How should the new development relate to existing development? What should happen at the edges of the development site?

The answer: Site access to Phase 2, 3 and 4 be gained off the main Spine Road to Phase 1. The existing road route provides good permeability and connections. A permeable pedestrian route has been incorporated to allow the adjacent housing scheme a choice of route along the desire line to access the proposed site.

2. Does the development provide (or is it close to) community facilities, such as shops, schools, workplaces, parks, play areas, pubs or cafes?

2a Are there enough facilities and services in the local area to support the development? If not, what is needed? Where new facilities are proposed:

2b Are these facilities what the area needs?

2c Are these new facilities located in the right place? If not, where should they go?

The answer: Yes the development is close to shops, and is on an existing bus route providing good links into the shopping hubs.

3. Does the scheme have good access to public transport to help reduce car dependency?

3a What can the development do to encourage more people (both existing and new residents) to use public transport more often?

3b Where should new public transport stops be located?

The answer: Yes, good rail access is available within the town of Clitheroe and provides links to Preston and Blackburn and further afield to Manchester.

4. Does the development have a mix of housing types and tenures that suit local requirements?

4a What types of homes, tenure and price range are needed in the area (for example, starter homes, family homes or homes for those down sizing)?

4b Is there a need for different types of home ownership (such as part buy and part rent) or rented properties to help people on lower incomes?

The answer: The scheme shows a wide range of homes. The majority of the dwellings are family homes with 3-5 bedrooms, however there are also houses with 2 bedrooms. This good mix will allow for down sizing and satisfy the local need for family homes. The affordable homes, that forms 30% of the total scheme will incorporate a mix of social rented and intermediate housing.

5. Does the scheme create a place with a locally inspired or otherwise distinctive character?

5a How can the development be designed to have a local or distinctive identity?

5b Are there any distinctive characteristics within the area, such as building shapes, styles, colours and materials or the character of streets and spaces that the development should draw inspiration from?

The answer: The scheme has been specifically designed to form distinct areas. Private drives with buildings fronting the green edge have taken an organic shape. The retention of the existing hedges and trees will allow the new development to tie into the naturalistic look of the site.

Design Responses

Building For Life

6. Does the scheme take advantage of existing topography, landscape features (including water courses), trees and plants , wildlife habitats, existing buildings, site orientation and microclimate.

6a Are there any views into or from the site that need to be carefully considered?

6b Are there any existing trees, hedgerows or other features, such as streams that need to be carefully designed into the development?

6c Should the development keep any existing building(s) on the site? If so, how could they be used?

The answer: Adequate spacing between development buildings along the boundaries will allow views through to the site. Additional landscaping between an existing hedge is proposed and will further enhance the linear buffer.

A stand off has been applied along the boundaries of the site to give an open feel with the new development set back.

7. Are buildings designed and positioned with landscaping to define and enhance streets and spaces and are buildings designed to turn street corners well?

7a Good streets and spaces are created by enclosing them with buildings and a strong landscaping scheme. Are buildings used to create enclosed streets and spaces?

7b Good buildings ‘turn’ corners. Do buildings turn corners well?

7c Do all fronts of buildings, including front doors, face the street?

The answer: The layout has been designed to echo the built form to the surrounding developments. This together with tree planting will enhance the street scape and present a well defined space. Where corners of housing development parcels exist double fronted house typologies have been employed with an appropriate size of building for the location. These double fronted buildings will have windows in the frontages facing the roads thus offering an active frontage to these important points. Existing public right of ways are retained and these spaces will be fronted by development providing natural surveillance.

8. Is the development designed to make it easy to find your way around?

8a Will the development be easy to find your way around? If not, what could be done to make it easier to find your way around?

The answer: Yes the street formation has been laid out to provide an obvious routes with building at vista nodal points providing visual way finding locators.

9. Are streets designed in a way that encourage low vehicle speeds and allow them to function as social spaces?

9a Are streets pedestrian friendly and are they designed to encourage cars to drive slower and more carefully?

9b Are streets designed in a way that they can be used as social spaces, such as places for children to play safely?

The answer: The edges of the development have a road formation that is tortuous in its horizontal alignment, this will reduce speed in these areas. Where the scheme has a more straighter road form raised junction tables may be employed to break the lengths of road where speed could be increased.

10. Is resident and visitor parking sufficient and well integrated so that it does not dominate the street?

10a Is there enough parking for residents and visitors?

10b Is parking positioned close to people’s homes?

10c Are any parking courtyards small in size (generally no more than five properties should use a parking courtyard) and are they well overlooked by neighbouring properties?

10d Are garages well positioned so that they do not dominate the street scene?

The answer: Parking has been dealt with in a range of ways depending on the location and amount service area required. The methodologies employed are: in curtilage front parking, in curtilage integral garage and gable parking between buildings. These methods have been incorporated in the appropriate situations. The scheme does have parking visible to the street, where this is the case heavy soft landscaping is proposed to screen and break up the rhythm of the parked car. Landscaping has been used to break the visual impact on key views and streets.

11. Will public and private spaces be clearly defined and designed to have appropriate access and be able to be well managed and safe in use?

11a What types of open space should be provided within this development?

11b Is there a need for play facilities for children and teenagers? If so, is this the right place or should the developer contribute towards an existing facility in the area that could be made better?

The answer: The majority of the scheme has been designed to maximise private space with these areas being well defined and legible to the home owner as well as the passing public. These areas have been demarked by soft landscape to aid the distinction between private and public space.

There are areas of public open spaces contained on the edge of the development.

12. Is there adequate external storage space for bins and recycling, as well as vehicles and cycles?

12a Is there enough storage space for bins and recycling, as well as vehicles and cycles?

The answer: The majority of the scheme is made up of family homes with gardens. It is proposed that the bin facilities for these properties will be contained within the private garden area of the associated dwelling. Vehicle and cycle storage has been incorporated to many of the plots by way of garages either integral or detached from the building.

Use and Amount

5.1 The site comprises of 426 residential units with associated parking. These units range from 2 bedroom properties up to 5 bedroom family homes.

5.2 The storey heights of the buildings range from 1, 2, 2.5 & 3 storey buildings in specific locations. This range in heights will give a definition to street termination points and give interest in roof scape.

5.3 The heights of the properties range from circa. 8 metres to 12 metres with varying widths of buildings.

5.4 The planning application site area is:

169158

sq m

41.8

acres

16.9

hectares

House Type	Bed No.	Type	No.	Sqft	Total Sqft
Affordables - Rent					
Apartments (gf elderly)	1	Block	2	562	1124
Apartments (ff/sf)	1	Block	4	562	2248
PA24 / Beckford	2	Semi	2	602	1204
PA24 / Beckford	2	Mid	8	602	4816
PA24 / Beckford	2	End	10	602	6020
PA34 / Gosford	3	Semi	14	866	12124
PA34 / Gosford	3	Mid	3	866	2598
PA34 / Gosford	3	End	6	866	5196
PT36 / Easedale	3	Semi	0	931	0
Bungalow 3	2	Semi	12	721	8652
Bungalow 3	2	Mid	1	721	721
Bungalow 3	2	End	2	721	1442
Total Affordables			64		43982
Affordables - Intermediate					
Apartments (gf elderly)	1	Block	2	562	1124
Apartments (ff/sf)	1	Block	4	562	2248
PA24 / Beckford	2	Semi	2	602	1204
PA24 / Beckford	2	Mid	9	602	5418
PA24 / Beckford	2	End	10	602	6020
PA34 / Gosford	3	Semi	18	866	15588
PA34 / Gosford	3	Mid	2	866	1732
PA34 / Gosford	3	End	4	866	3464
Bungalow 3	2	Semi	10	721	7210
Bungalow 3	2	Mid	1	721	721
Bungalow 3	2	End	2	721	1442
Total Affordables			64		46171
Open Sale					
Apartments (gf elderly)	2	Block	4	655	2620
Apartments (ff/sf)	2	Block	8	655	5240
Bungalow 3 (Elderly)	2	Semi	22	721	15862
Bungalow 3 (Elderly)	2	Mid	2	721	1442
Bungalow 3 (Elderly)	2	End	4	721	2884
PA34 / Gosford	3	End	5	866	4330
PA34 / Gosford	3	Mid	5	866	4330
PA34 / Gosford	3	Semi Detached	23	866	19918
PT36 / Easedale	3	End	1	931	931
PT36 / Easedale	3	Semi Detached	13	931	12103
PD30 / Amersham	3	Detached	24	990	23760
PA42 / Lydford	4	Detached	26	1099	28574
NB31 / Braxton	4	Semi Detached	68	1091	74188
NT41/Trusdale	4	Detached	44	1243	54692
ND40 / Coltham	4	Detached	23	1259	28957
NT42 / Waysdale	4	Detached	8	1549	12392
PD51 / Lavenham	5	Detached	18	1646	29628
Total Open Sale			298		321851
Total			426		412004



The Scheme

Layout and Scale

Layout

5.5 The design has responded to existing constraints and opportunities on site whilst also aiming to provide an efficient housing scheme utilising good design. The degree of sensitivity of residential development surrounding the site is varied due to three considerations: the distance between the properties and the site boundaries, the topography of the surroundings, and the extent of tree coverage along site boundaries. The design therefore aims to deliver the following aspects to the proposed development making residential areas safer and more attractive:

- Reducing the impact of traffic in housing areas by removing cars from the road by ensuring each house has off street parking.
- Ensuring that new housing is of a high standard and represents an improvement to the local environment by establishing a set of house designs that reflect modern living design standards whilst being visually attractive.
- Every new home has been allotted its own secure and private garden together with in curtilage parking.
- Security has been a major consideration when designing boundary treatments and positioning windows, and as a result, blank side gables on street corners have been minimised with windows providing passive surveillance within the development. Boundary treatments are discussed in detail under the landscaping section of this document.
- Informal open space have been set on the boundaries of the development and buildings have been turned to face these areas to provide pleasing outlooks into and out of the site.
- The layout of the site has been produced to optimise the existing vistas, achieve the legislated DPH, be clearly legible, improve connectivity of the site and local area, provide natural surveillance and create an attractive and distinctive development.
- Open space has been specifically designed to be overlooked by properties to ensure safe use for all residents.



Proposed site layout - Phase 2, 3 & 4

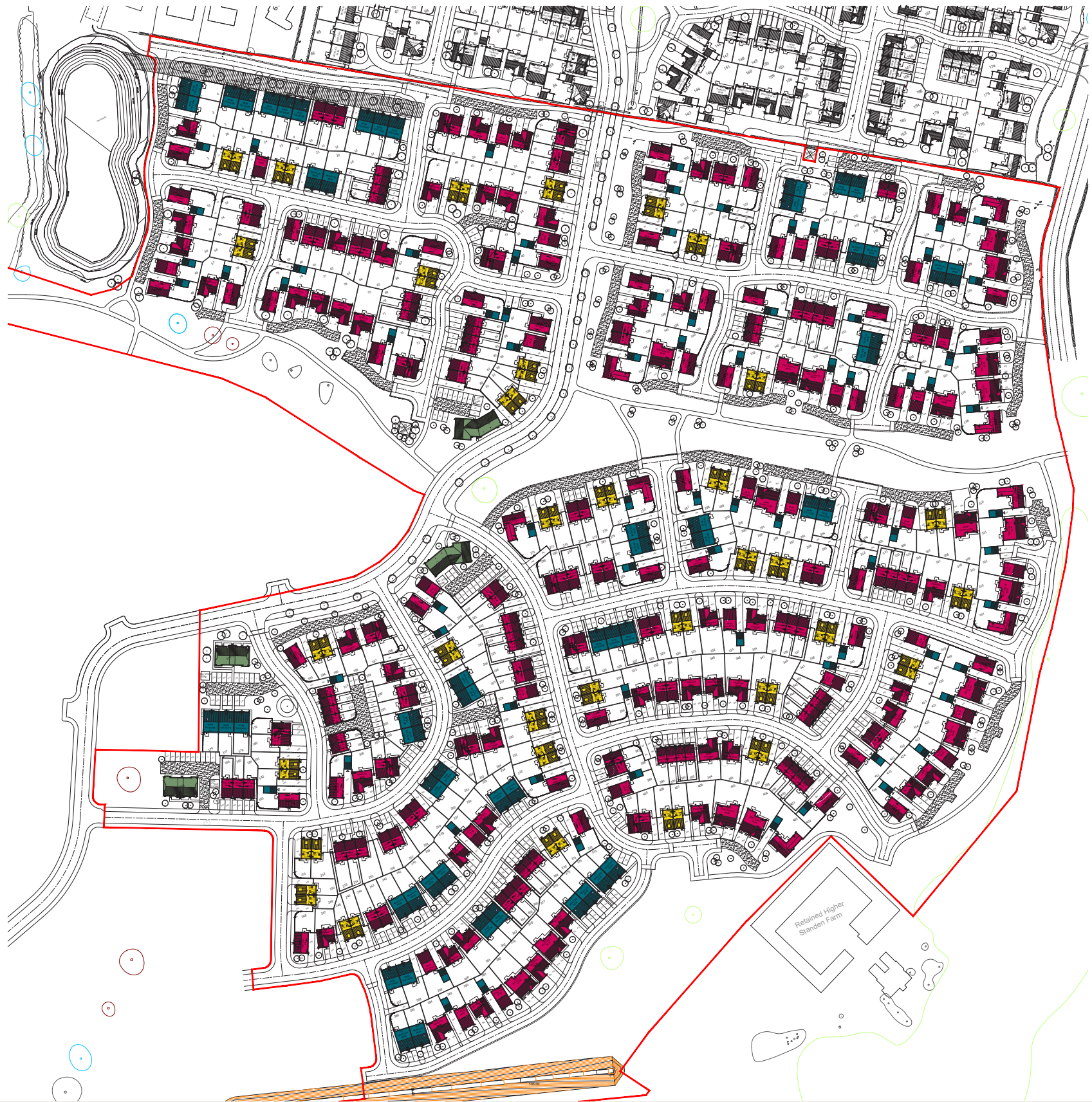
Layout and Scale

Scale

5.6 The scale of building is of importance if the development is to integrate into the existing townscape. With this in mind, the massing and rhythm of the surrounding buildings has been repeated within the proposed development, replicating the precedent set in the local area. This consists of a range of 2, 3, 4 and 5 bedroom houses all of 1, 2, 2.5 & 3 storeys. Hard and soft landscaping features have also been chosen to reflect the scale of the surrounding area. There are also some apartments at 3 storey.

LEGEND

- 1 Storey Buildings
- 2 Storey Buildings
- 2.5 Storey Building
- 3 Storey Buildings



Proposed Storey Heights Plan

The Scheme

Character Areas

5.7 Within the proposal, the design has employed the use of character area zones to enhance a legible layout form. The use of character areas is important to provide a sense of place and to define routing of public realm. Below are some examples of how these character areas have been defined within the scheme.

-  Village Street character area
-  Development Edge character area
-  The Lanes character area



Character Area Location Plan

Village Street

5.8 This character area is formed by the development's road alignment. The street is designed to extend to the southern boundary for potential future development. 2 metre wide pedestrian footways and 3.5 meter wide cycleways are intended to run either side of this road to allow the public realm to filter through the site.

5.9 The layout has been designed to front this road offering an attractive street scene and natural surveillance to a well trafficked route by both car and pedestrian. 2 and 2.5 storey buildings have been incorporated into the street scape offering a varied roof line and adding visual interest.

5.10 Careful consideration has been given to the materials along this zone, providing a varied palette to the architectural form. All properties have their own designated parking space to help keep cars off the road.



Village Street location



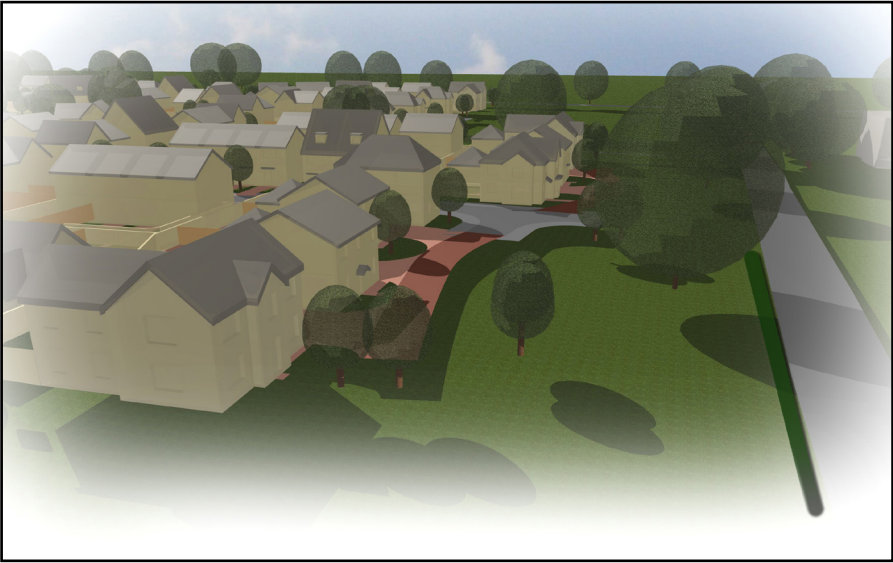
Typical Village Street, street scene

The Scheme

Development Edge

5.11 The development edge forms a transitional edge between the open space and new development. Buildings will front onto the green infrastructure. Utilising the existing landscape boundary of the site, the green edge settlement will tie the existing trees and hedges into development retaining views and links into the established landscape whilst providing natural surveillance.

5.12 The layout has been planned to maximise the larger properties along these edges giving a lower density at the periphery of the development.



Indicative example of Development Edge street view



Development Edge locations



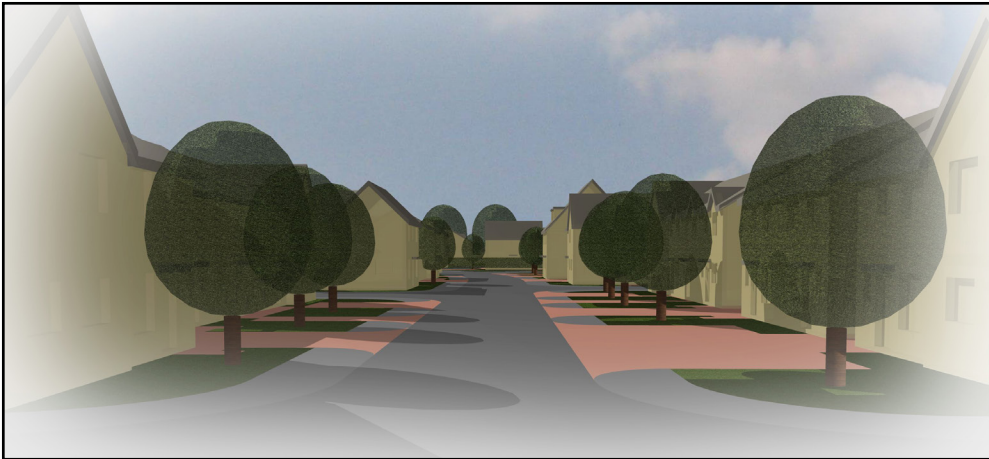
Typical Development Edge street scene

The Scheme

Lane character area

5.13 This character area is again formed by the development’s road alignment. The street is a 5.5m wide road. Two metre wide pedestrian footways are intended to run either side of this road to allow the public realm to filter through the site.

5.14 The layout has been designed to front the road offering an attractive street scene and natural surveillance to a well trafficked route by both car and pedestrian. All properties have their own designated parking space to help keep cars off the road and as a result the street scene will be less cluttered.



Indicative example of the street view



The Lane locations



Typical Lane street scene

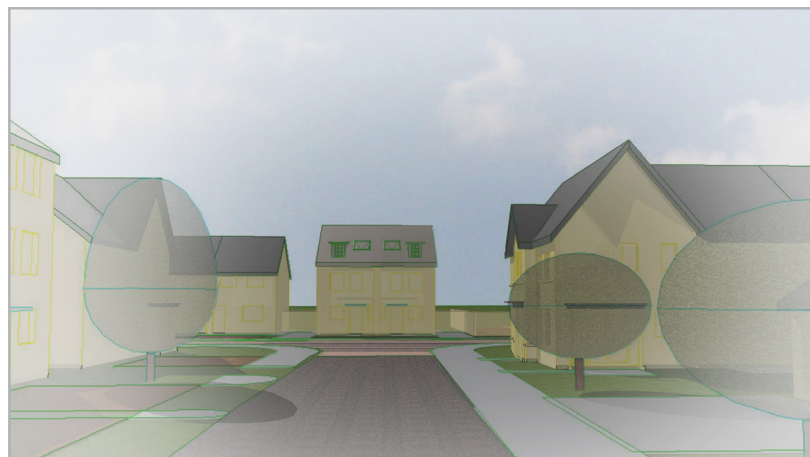
Visual Links

5.15 The scheme has been laid out with connecting views between the existing routes and the open space beyond. This allows the development to have a visually permeable appearance and to sit well within its setting.

5.16 Where key vistas are presented at the head of avenue the massing of the proposed building have been increased to provide a feature and visual relief. These buildings manifest themselves as linked 2 & 2.5 storey houses.

The Focal Points

5.17 These areas will be marked with key buildings, set apart from the other buildings within the development. Carefully chosen materials and styles that typify the area of Clitheroe will lighten and mark the arrival points and way finding points within the scheme.



Typical example of vista views within the development



Example dual fronted house type



Visual Links Location Plan

The Scheme

Movement and Connections

Road pattern

5.18 The site plan has been designed to allow the development to be easily navigated and with key permeable routes. A main street has been employed in the development which could provide access to the potential development site to the south. This main street is a 6.75m wide road. The 6.75m road extends off this main street in certain locations to serve the whole site, leading to 4.5m secondary road. This will allow large vehicles to penetrate the site and easily navigate through it.

5.19 Secondary connecting roads have been employed to allow a further permeable road pattern allowing the user to navigate through the development with ease.

Pedestrian movement

5.20 Pedestrians have clearly defined routes throughout the scheme with designated footways separating them from road traffic along the main pedestrian public realm. These routes will be safe and comfortable spaces well over looked by the proposed buildings.

Buildings

5.21 The proposed buildings will aid movement assisting with navigation from key locations. Feature plots have been employed to ensure a sense of place and focal points have been positioned to provide distinct points along the routes. These points will be memorable locations and provide the scheme with its distinct character.

5.22 The scheme will incorporate bin facilities for the houses.

Indicates Primary vehicular route

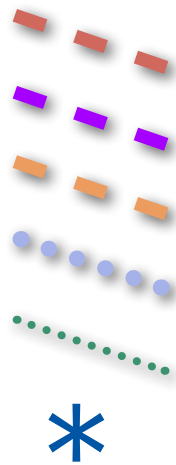
Indicates Secondary vehicular route

Indicates Tertiary Road

Indicates proposed Shared Access

Indicates pedestrian route

Indicates vehicular turning platform



Movement and Connections Plan

The Scheme

Landscaping

5.23 The hard and soft landscaping in this scheme is designed to create a high quality public realm that is sympathetic to the surrounding area whilst maintaining the unique identity and character of this proposed development. The lifespan of the landscaping will be of critical importance when considering surface finishes and planting if longevity is to be achieved. The landscaping will provide texture and make the development feel established. Careful choice of plants will significantly improve the biodiversity of the site and provide a habitat for urban dwelling wildlife. Landscaping and the street furniture have been given significant weight at the inception stage to insure it is fully integrated.

5.24 As part of the landscape proposals for the area of public open space to the south of phase two, some trim trail play equipment as well as benches and bins will be provided. This will ensure that the open space is actively used by members of the public. A pedestrian link from Worston Old Road to Shays Drive has also been provided through the public open space.

5.25 The site is bounded by Pendle Road Phase 1 and Worston Old Road where the road is characterised by landscape buffering. Existing trees and hedge planting along these boundaries have now been retained and will provide a naturalistic edge to the development.



Examples of boundary treatments



Existing hedge to the development northern boundary facing Pendle Road



Boundary Treatment Layout

5.26 The site's existing hedgerow and trees running along boundaries will give the development a sense of maturity, whilst restoring the site's landscape inheritance and to ensure natural habitat is preserved. All buildings and roads are set outside the root protection line to continue the longevity of these features.

5.27 All intermediate plot boundaries will consist of 1.8 metre high timber fencing with concrete posts and panels to satisfy the security and privacy of each dwelling. 1.8 metre high timber fencing will form the rear boundaries.

5.28 Boundary brick walls will incorporate piers and timber panels, partially shielded by planting with climbing species to provide a secure but visually appealing barrier within the development's street scene.

5.29 The development offers several visual links allowing views to the wider landscape through the development. The road pattern along these links are kept generally parallel with the build line open to maintain clear views and visually encourage the eye along the road to the back drop of the fields beyond.

The Scheme

5.30 The development offers several visual links allowing views through the development to the existing fields beyond. The road pattern along these links are kept generally parallel with the build line open to maintain clear views and visually encourage the eye along the road to the back drop of the existing tree canopies.

5.31 The road structure within the scheme incorporates areas of parking integrated within raised tables. The raised tables will be surfaced with coloured tarmac, however, to ensure that the parking bays are clearly legible in the street, a contrasting colour will be used to outline the individual parking bays.

5.32 A mixed palette of materials will be used on the general road surfacing. The main streets will be tarmac, broken down with the coloured tarmac raised tables. Some secondary roads will be surfaced in tarmac with coloured chippings, which not only softens the impact of these roads but also acts as clear delineation between the primary and secondary streets.



View along Pendle Road

The Scheme

Appearance

Village Street Character

5.33 The Village Street will be the primary route through the site for vehicular traffic. The Village Street forms the gateway character.

Character Theme:

5.34 Traditional Village Street. The character area should include styles of development referencing back to pre and post industrial revolution to form the evolving Village Street.

Character Justification:

5.35 Representative of the historic expansion of village and its transition to an industrial hub.

Building Types and Heights:

5.36 2 to 2.5 storey blocks. Primarily terraces with landmark buildings with a unique individuality.

Building Type Justification:

5.37 Buildings of the industrial revolution / 19th century workers' cottages as common in surrounding villages.

Typical Building Features:

5.38 Vertical emphasis clear in the proportions of windows. Windows will be an eclectic mix of styles and colours. Brick or stone quoins, stone heads and cills or stone window and door surrounds which could be painted. Brick/stone chimney stacks used where appropriate.

Building Materials:

5.39 Brick, stone, render walls with a predominance of slate look roof, interspersed with limited / occasional brown plain tiles.



Waddington Village - Example for Clitheroe Village Streets



MATERIALS:

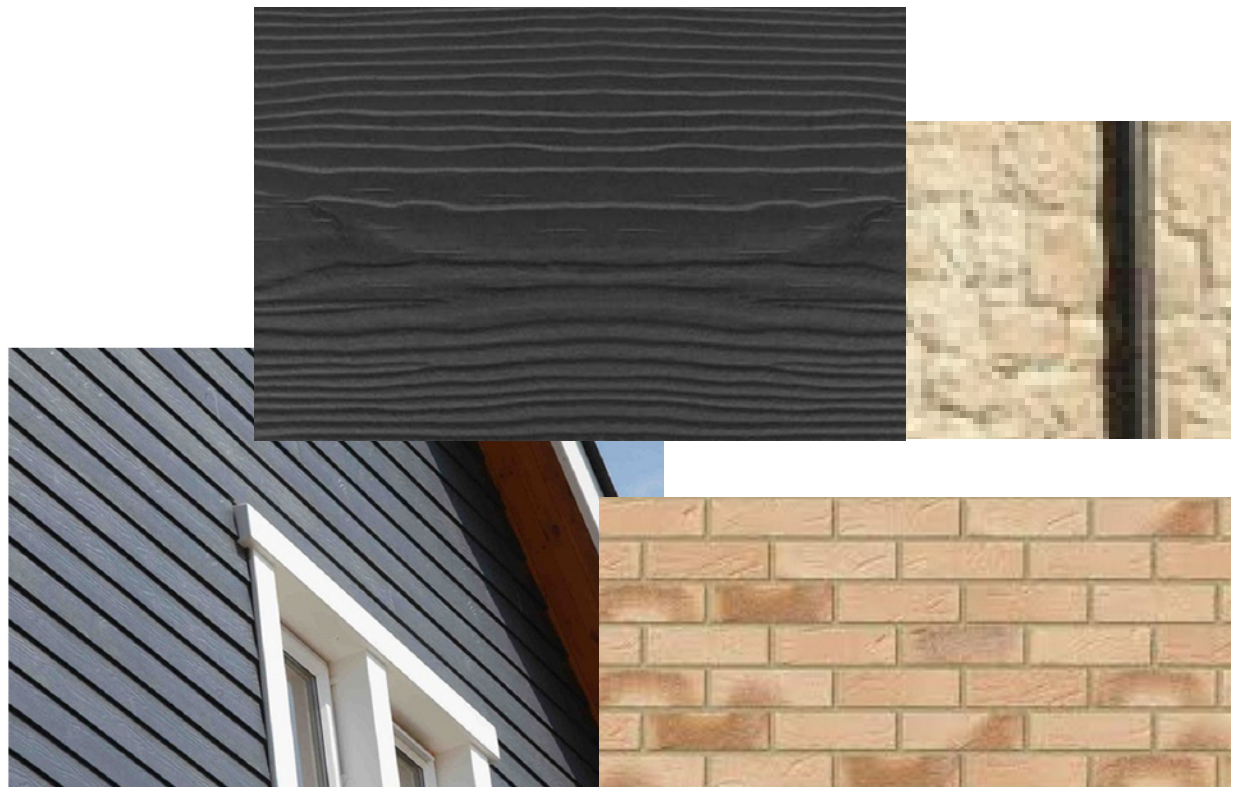
- Forterra village harvest multi Buff Brick.
- Reconstituted stone (Marshalls Cromwell Rustic Weathered).
- Render (Natural/Cream colour).
- Cedral Lap Weatherboarding.
- Russell Grampian - Grey roof tiles.
- Grey uPVC window frames no glazing beads
- Grey soffits/fascias.
- Black RW Goods.

DETAILS:

- Boxed eaves detail
- Art stone surrounds to windows/doors.
- Canopies over doors (where applicable).
- Dummy chimneys to focal locations.



Typical Building Features



Appearance

Development Edge Character

5.40 The Green Edges will form a softened edge to the development parcels which front onto the existing countryside. The build line should be fluid with large gaps between buildings to reduce the impact of development.

Character Theme:

5.41 The Green Edges should reflect the lowest density on the site and therefore be larger, more exclusive properties with a feeling of spaciousness, overlooking the existing countryside.

Character Justification:

5.42 The Green Edges are the interface between the existing countryside and the development behind, These spaces should therefore be visually permeable to enable views out towards Clitheroe landmarks.

Building Types and Heights:

5.43 1 to 2.5 storey spacious detached properties.

Building Type Justification:

5.44 An eclectic array of building styles and materials sensitive to their natural setting whilst promoting individuality along the Green Edge.

Typical Building Features:

5.45 Greater variation in window proportions, Render and stone facades with stone quoins, stone heads, cills surrounds and mullions, and feature door surrounds. Stone chimney stacks used where appropriate.

Building Materials:

Stone, render walls with typically slate style roofs.



Examples of Green Edges around Clitheroe

MATERIALS:

- Reconstituted stone - Marshalls Cromwell Pitched
- Face Weathered.
- Render (Green).
- Russell Lothian - grey roof tiles
- White uPVC window frames.
- White soffits/ fascia's.
- Black RW Goods.

DETAILS:

- Open eaves detail
- Art stone heads and cills to windows.
- Canopies over doors (where applicable).
- Art stone Quoin detail.
- Dummy chimneys to focal locations.



Typical Building Features

Lane Character

5.46 The Lancashire Village will be the character applied to secondary roads throughout the site. These areas provide opportunities for varying styles and approaches to be adopted to form an eclectic village style which gives the impression of natural expansion over time. Street layouts can also be altered throughout this character to further emphasise the different styles and eras expressed.

Character Theme:

5.47 Village expansion. The character area should include styles of development from different eras and design inspirations including Arts & Crafts, Georgian streets and Victorian workers cottages.

Character Justification:

5.48 Representative of the historic expansion of Lancashire villages. It is important to portray vernacular architecture throughout the Lancashire Village, forming unpretentious, simple and traditional places reflective of a built environment which has been continually influenced by preceding buildings.

Building Types and Heights:

5.49 1 to 2.5 storey blocks. Mews and detached properties are acceptable in this character area.

Building Type Justification:

5.50 Eclectic streets as common throughout Clitheroe, an example would be Hawthorne Place.

Typical Building Features:

5.51 Greater variation in window proportions, brick or stone quoins, stone heads, cills, surrounds and mullions, and feature door surrounds, fanlights, bay/bow windows, brick/stone chimney stacks used where appropriate.

Building Materials:

Brick, stone, render walls with typically slate style roof tiles or red / brown plain tiles.



Examples of Lancashire Village Character in Clitheroe

MATERIALS:

- Forterra village harvest multi Buff Brick.
- Wienerberger heartland light multi Red Brick.
- Render (Natural/Cream colour).
- Russell Grampian - Brown roof tiles.
- Russell Grampian - Grey roof tiles.
- White uPVC window frames.
- White soffits/ fascia's.
- Black RW Goods.

DETAILS:

- Boxed eaves detail
- Vousoir brick heads and double cill detail to windows contrasting colour - brown/red).
- Canopies over doors (where applicable).
- Dummy chimneys to focal locations.



Typical Building Features

The Scheme

Appearance

- 5.52 The development site is deemed as the gateway to Clitheroe. The proposed houses will include detailing that will convey the character of the area taking inspiration of key architectural features.
- 5.53 The use of high quality materials will give longevity to the development helping to maintain an attractive appearance. It is considered that this will encourage people to take pride in their properties.
- 5.54 The buildings will be detailed according to their position and aspect on the site with features being added on key focal points and vistas. Additional windows will be added where sight lines exist or to break up exposed blank walls.
- 5.55 Within the proposal the design has employed the use of character area zones to enhance a legible layout form. The use of character areas is important to provide a sense of place and to define routing of public realm. The areas have been broken down into palatable sized sections, as shown on the coloured key plan and accompanying images.

Roof tiles

Grey
Brown

Windows

White and Anthracite 7016

Drainage

Black half round rainwater goods
White fascia boards

Material Legend

(Refer to the house type drawing for specific details)

Facing & details	Painted gas & electric box RAL colours	Roof type
<div>Material Palette 01 Walling Stone - Marshalls Cromwell Pitched Face Weathered and Artstone detailing.</div>	<div>RAL 1000</div>	<div>● Roof Tile 01 - Russell Grampian - Brown</div>
<div>Material Palette 02 Buff Brick - Forterra Village Harvest Multi and Artstone detailing.</div>	<div>RAL 9005</div>	<div>● Roof Tile 02 - Russell Grampian - Grey</div>
<div>Material Palette 03 Red Brick - Wienerberger Heartland Light Multi Detail - Blockley Class B Red Brick.</div>		<div>▲ Roof Tile 04 - Russell Lothian - Grey</div>
<div>Material Palette 04 Buff Brick - Forterra Village Harvest Multi Detail - Wienerberger Staffordshire Smooth Brown</div>		
<div>2cs Below DPC - Wienerberger Staffordshire smooth blue for all plots (except apartments).</div>		
<div>Material Palette 06 - Apartments Combination of reconstituted stone, render, slate grey weatherboarding with Artstone detailing. For details, refer to drawings TW/HSF/HT/60-64.</div>	<div>All meter boxes to side. Mids to have front electric box and semi concealed gas box.</div>	
<div>Vent/flue colours: Red brick - Brown vents & flue Stone & Buff - White vents & flue</div>		
		Render / Boarding
		<div>— Render 01 - Ivory</div>
		<div>- - - Render 01 - Ivory (Upper floor render - Refer to elevations)</div>
		<div>— Render 02 - Green (Parex - Monorex - Scraped V30 Pale Green)</div>
		<div>- - - Render 02 - Green (Upper floor render - Refer to elevations) (Parex - Monorex - Scraped V30 Pale Green)</div>
		<div>— Weatherboarding (Slate grey)</div>



Materials Layout

Summary

6.1 This Design and Access statement is in support of a reserved matters Planning Application for 426 new dwellings, associated parking and landscaping on Phases 2, 3 & 4 of the development.

6.2 The scheme has been designed as a natural extension to the current development taking place on phase one. The elevations of the house types utilise similar designs and materials. It also represents a high quality residential development that will compliment the local area.

Secure Development

6.3 The detailed site analysis and extensive design process leading to a layout form that is developable and acceptable within the character of the area, whilst maximising natural surveillance throughout.

Quality housing stock

6.4 The scheme uses house types that achieve national space standards. The house types are standard Taylor Wimpey house types and are therefore safe and efficient to build. The elevations are bespoke to this scheme and therefore their appearance reflects the character of Clitheroe.

Accessibility

6.5 All dwellings will comply with the requirements set within Part M of the British Building Regulations.

Enhanced Environment

6.6 The built form within the development will sit comfortably within its surrounding, whilst providing an active street scene and sense of place. The proposed street scenes have been designed to knit well with the existing built environment, and improving the appearance whilst working within the grain and pattern of the surrounding area.

Integrated Parking

6.7 Each dwelling has a designated off street car parking space set to be convenient for the home owner with minimum travel distances from the related dwelling. Where possible the car parking has been designed to be within plot curtilage to enhance ownership and maintenance. The car has a critical impact upon any new development and as such, and where possible, the car has been located off the street scene.

Private Space

6.8 Each dwelling within the development will accommodate adequate private family gardens. Each garden will be given a paved area for bin storage with simple routes to public highway.



Proposed site layout

**Taylor
Wimpey**

Baldwin Design Consultancy Ltd