

Mr N Forshaw

Proposed Change of Land-Use,  
Bolton Fold Farm, Alston Lane, Longridge

# Transport Statement

3707323



AUGUST 2023

## Report Details

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**Report Number:** 3707323-TS

**Report Title:** Transport Statement, Proposed Change of Land-Use, Bolton Fold Farm, Alston Lane, Longridge

**Client:** Mr N Forshaw

**Date:** 04 August 2023

**Status:** -



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# 1 Introduction

## 1.1 General

Dynamic Transport Planning Ltd is instructed by Mr N Forshaw to report on the anticipated highways and transportation considerations related with a retrospective application for a change of land-use from existing agricultural buildings to a mixture of B2/B8, E-Class and *Sui Generis* uses, at Bolton Fold Farm, Alston Lane, Longridge.

This Transport Statement (TS) has been produced to advise Ribble Valley Borough Council (RVBC) as the Local Planning Authority and Lancashire County Council (LCC) as the Local Highways Authority on the transport planning considerations associated with the development.

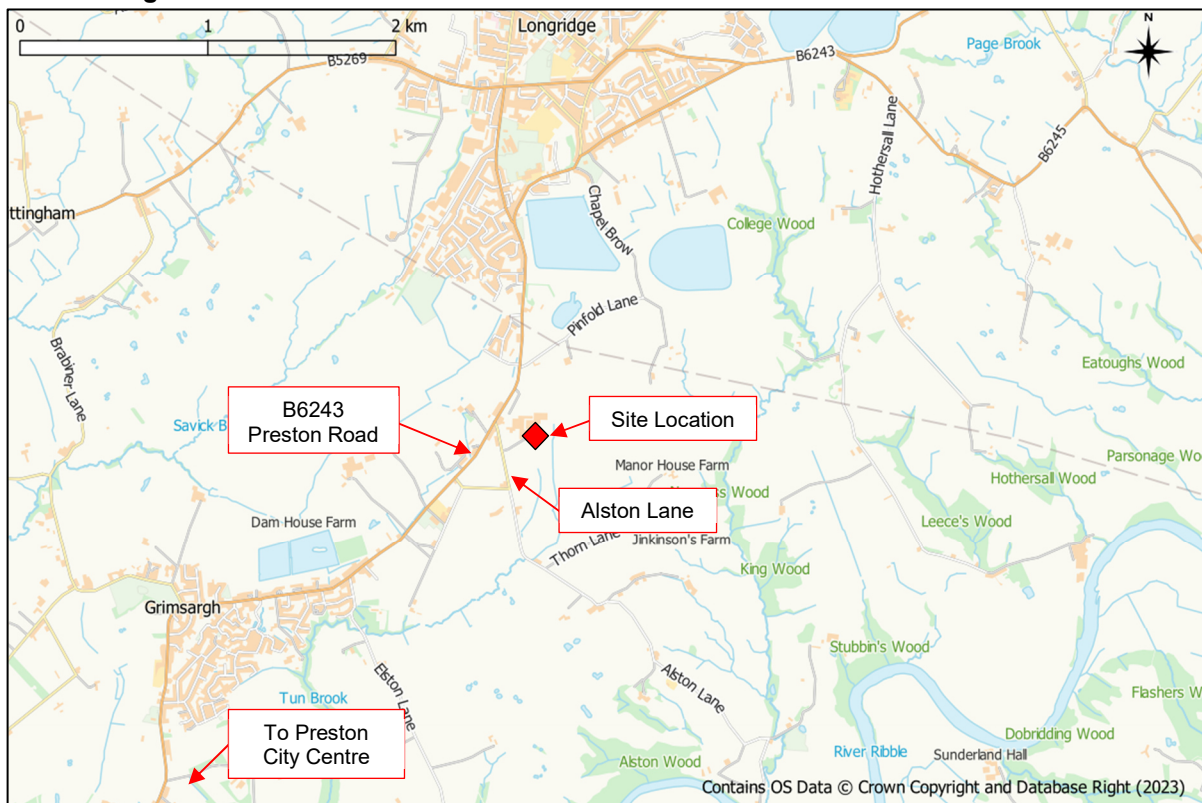
## 1.2 Site Location

The wider Bolton Fold Farm site comprises various agricultural buildings associated with Alston Dairy, including a farm shop and café.

The application site is located on land situated to the south-east of Bolton Fold Farm, approximately 2km north-east of Grimsargh Village, around 2km south-west of Longridge Town Centre and approximately 8.5km north-east of Preston City Centre.

The location of the site is illustrated at **Figure 1.1:**

**Figure 1.1 – Site Location Plan**



### 1.3 Planning History

Bolton Fold Farm has been subject to various approved planning applications for agricultural buildings and facilities associated with Alton Dairy, alongside residential dwellings.

In 2021, a planning application for a new storage building (Unit 2), for animal feed (hay/straw) and machinery (ref. 3/2021/0090) was made in 2021, where it was decided planning permission was not required.

An additional planning application was submitted in 2022, for a similar development for two new storage buildings, Units 1 and 3, to provide storage for animal feed and machinery (ref. 3/2022/0208).

The current application is for a retrospective change of land-use for these existing agricultural buildings to B2/B8, E-Class and *Sui Generis* uses.

### 1.4 Pre-application Discussions

Pre-application discussions were undertaken with RBVC where the following points were raised regarding highways matters:

- Footways within the vicinity of the site;
- Suitability of the existing vehicle access to the application site and potential conflicts with other vehicles accessing/egressing the site; and
- On-site parking capacity, turning facilities/provision of electric charging and cycle storage.

### 1.5 Purpose and Structure of Report

This TS will review the site layout including swept-path analysis, detail the trip generating potential and review the accessibility of the site.

In accordance with best practice, this TS has been prepared in reference to the Department for Transport (DfT's) '*Guidance on Transport Assessment*' document.

This report is structured as follows:

- Section 2 provides a reference to relevant planning policy;
- Section 3 sets out the existing conditions of and around the application site;
- Section 4 provides a description of the site;
- Section 5 assesses sustainable access options;
- Section 6 outlines the trip generating potential of the site; and
- Section 7 presents the summary and conclusions.

## 2 Existing Conditions

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### 2.1 General

The application site is located approximately 2km north-east of Grimsargh Village, comprising three existing agricultural storage units, situated on land south-east of Bolton Fold Farm. The units have a combined approximate Gross Floor Area (GFA) of 837m<sup>2</sup>.

The site is bounded by Bolton Fold Farm/Alston Dairy to the north, agricultural land to the south and east and the site access lane to the west. The surrounding area of the site largely comprises agricultural and undeveloped land, with the B6243 Preston Road running to the west of the wider farm site.

Access to the site is taken via an informal priority-controlled 'T' junction off Alston Lane, to the south-west of the site. The site is separated from Bolton Fold Farm by a barrier adjacent to the north-western corner of the site, where a segregated farm access lane runs parallel to the site access lane.

### 2.2 Surrounding Road Network

#### 2.2.1 Alston Lane

Alston Lane runs in a general north/south orientation within the vicinity of the site. The single lane road is situated to the west of the development site, where access and egress to the application site is taken. The road is lightly trafficked, as demonstrated by the survey data shown at **Appendix 1**.

To the north-west of the site, Alston Lane connects to the B6243 Preston Road via a two-way priority-controlled junction. To the south of the site Alston Lane connects to various unnamed farm access roads. The road is approximately 5m in width and subject to the national speed limit.

#### 2.2.2 B6243 Preston Road

Formed of a single carriageway, Preston Road runs to the west of the site in a general north-west/south-east orientation.

Within the vicinity of the site, Preston Road is subject to a 40mph speed limit and has an approximate width of 7.5m. A right-hand ghost-island turning provides access to Alston Lane to the north-west of the site. The road benefits from street lighting and is supported by pedestrian footways along both sides of the carriageway.

### 2.3 Road Safety

Accident statistics have been obtained for the local highway network in the vicinity of the application site for the three years data covering 2019 to 2021 from the Department for Transport (DfT).

Within the assessment period no incidents were recorded along Alston Lane, or within the vicinity of the site access arrangements. Additionally, no accidents were recorded along the B6243 Preston Road, in the vicinity of the junction with Alston Lane.

On review of the data, it is considered that the existing accident record does not demonstrate any pre-existing patterns or trends of incidents that could be affected by the application site.

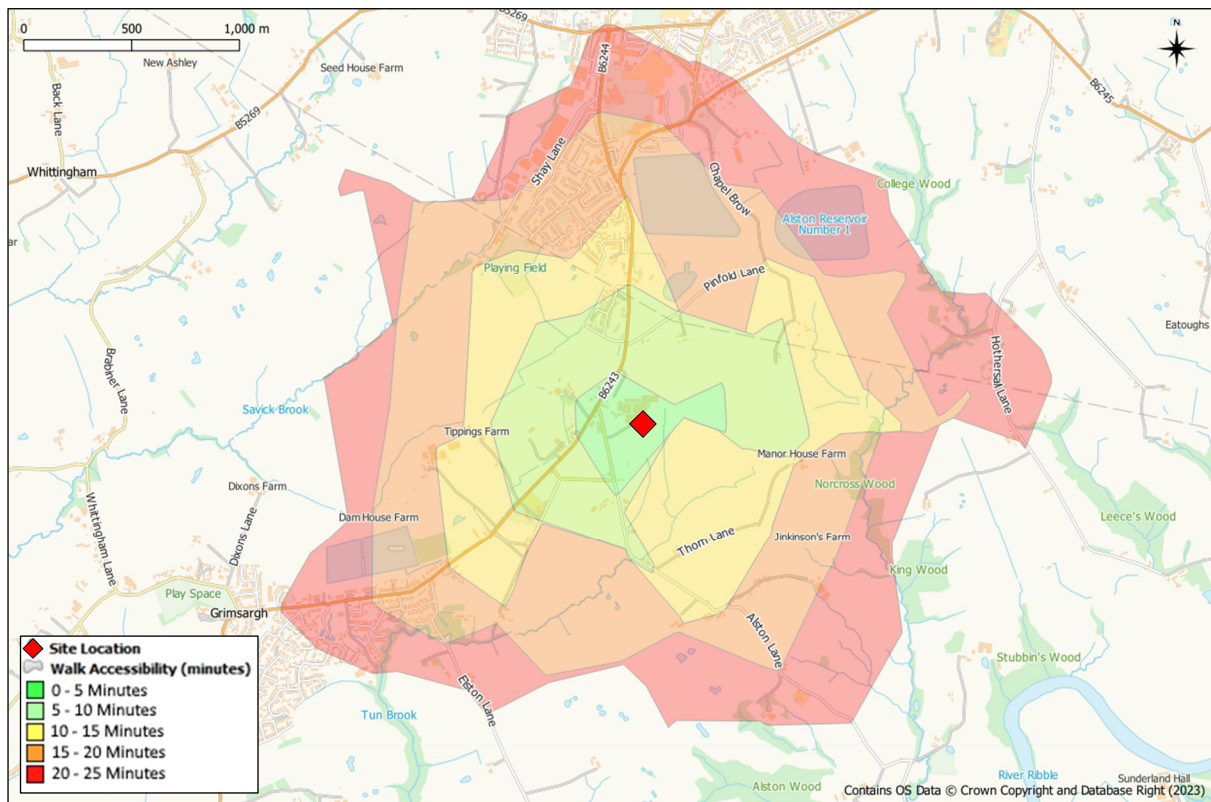
## 2.4 Access by Non-Car Modes

Notwithstanding the primary function of the development, in accordance with planning policy a review of access by all modes of travel has been undertaken. The options for accessing the site by sustainable modes are discussed in more detail below. As a retrospective application, the site is already operational. The applicant has confirmed that several of the site users do access on foot.

### 2.4.1 Pedestrian Access

Relevant guidance provided in Manual for Streets (MfS) states that walking offers the greatest potential to replace short car trips, particularly those under 2km. A walk accessibility plan provided at **Figure 2.1** highlights the accessible walking area within a 2km catchment of the site, equivalent to 25 minutes.

**Figure 2.1 – Walk Accessibility Plan**

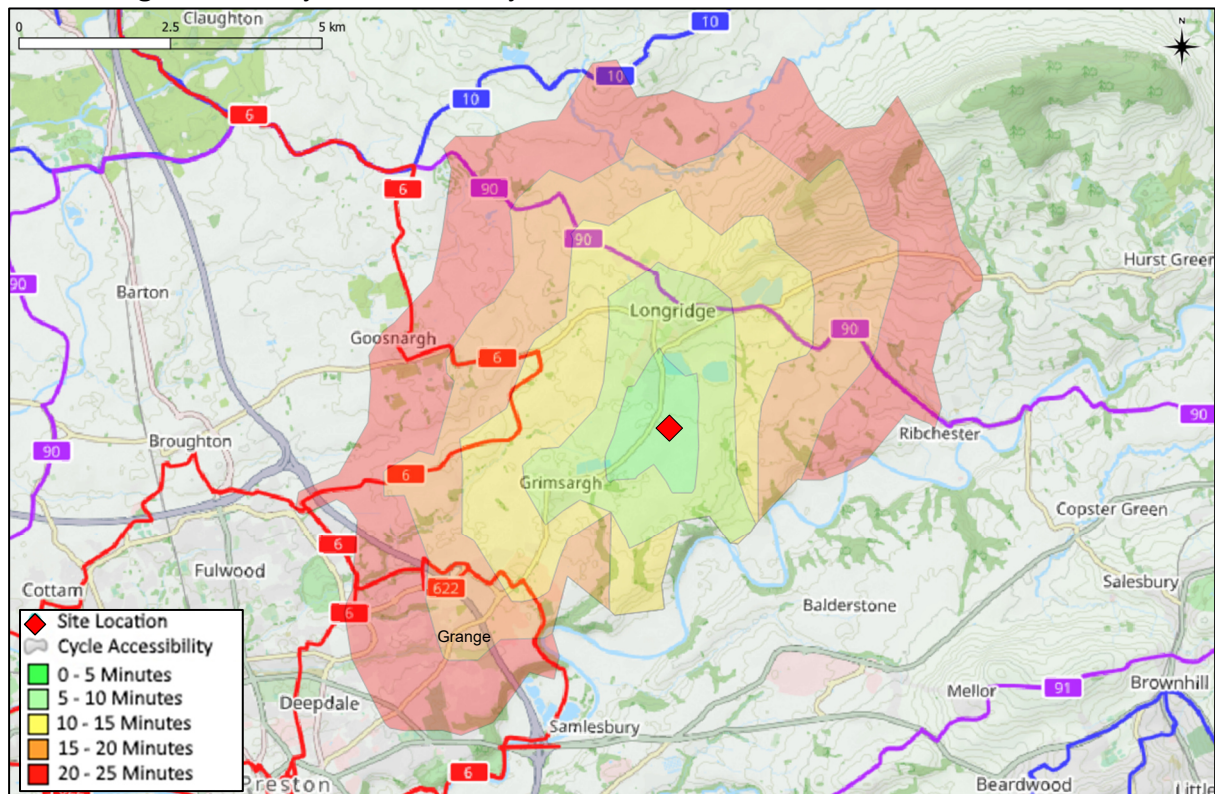


Alston Lane leading to the site provides a footway from its junction with Preston Road for 50m, with the next 100m section not having a footway. However, the road is lightly trafficked, as demonstrated by the survey data shown at **Appendix 1**. The plan indicates that the southern extent of Longridge and the eastern portion of Grimsargh are both accessible within a 25-minute walking distance of the application site.

## 2.4.2 Cycle Access

It is widely recognised that cycling has the potential to substitute shorter car journeys for work purposes. The areas reachable within a 25-minute cycle journey are identified within **Figure 2.2**:

**Figure 2.2 – Cycle Accessibility Plan**



The plan indicates that to the entirety of Longridge and Grimsargh are accessible within a 10-minute cycling journey of the site. To the south-west of the site, the north-eastern extent of Preston is accessible within a 20-minute journey, including the Grange residential area.

National Cycle Network (NCN) Route 90 is accessible within a 10-minute cycling journey to the north of the site, routing through Longridge. NCN Route 90 is a cycle loop which locally acts as a link route to Whalley to the north-east and NCN Route 6 to the north-west.

### **2.4.3 Bus**

The nearest bus stops are located approximately 300m to the north-west of the site, along the B6243 Preston Road, equivalent to around a four-minute walking journey. The no.1 bus serves these bus stops, which runs from Preston City Centre to Longridge every 10 minutes.

## **2.5 Accessibility Summary**

Overall, notwithstanding the primary function of the site, in accordance with planning policy, the accessibility by non-car mode has been assessed. It can be summarised that the site offers potential for journeys undertaken by users of the development by sustainable modes of travel.

## 3 Proposed Development

### 3.1 General

The proposals comprise of a change of land-use of three existing agricultural storage buildings to B2/B8, E-class and *Sui Generis* uses, where the dominant use class is anticipated to be B8 storage and distribution.

The application site is supported by vehicle parking facilities which will be retained in their current form. The proposed layout of the site can be found at **Appendix 2** of this report.

Information provided by the applicant highlighted that a significant portion of the units are currently used for storage and therefore, are not accessed on a regular basis, limiting the daily movements at the site, which is evidenced by the survey provided at **Appendix 1**.

### 3.2 Site Access Arrangements

The existing vehicular access arrangements will be retained to serve the commercial units. To the south-west of the site an informal priority-controlled junction off Alston Lane provides access to the segregated farm access and site access lanes, shown in **Figure 3.1**. The site access lane is situated to the south of the access arrangement, where a 20mph speed limit is in place.

**Figure 3.1** – Site Access Arrangements



Servicing and deliveries associated with the proposals will be retained as existing, with servicing undertaken to the rear of the building within the designated servicing area. A swept-path assessment for a 10m rigid truck is provided at **Appendix 2**.

### 3.3 Parking Provision

As part of the development, an unallocated parking area of hardstanding is provided for use by all units. As outlined earlier in this report, the majority of the units are used for storage and are not accessed on a regular basis.

Given the nature and location of the application, the parking provision is considered appropriate as each unit will have dedicated parking to allow access to all 10 Units when required.

## 4 Traffic Survey and Trip Generation

### 4.1 General

This chapter provides a survey of the trip generation for the operational site to demonstrate the actual levels of trip generation associated with the ongoing operation of the site.

Information provided by the applicant highlighted that a significant portion of the units are currently used for storage and therefore, are not accessed on a regular basis, limiting the daily movements at the site. This is reflected in the survey data outlined below.

### 4.2 Site Visit

A site visit was undertaken on the 15<sup>th</sup> of June 2023, with a survey of vehicle movements at the site access undertaken between 08:00 – 09:00 and 16:30 – 17:30.

#### 4.2.1 Traffic Survey Data

During the site visit, staff and visitors accessing and egressing both the application site access lane and the Bolton Fold Farm access lane were recorded. The multi-modal traffic survey at the site is shown in **Table 4.1**, with the full survey data shown at **Appendix 3**.

**Table 4.1** – Traffic Survey

Transport Mode	Application Site Access				Bolton Fold Farm Access			
	AM 08:00 - 09:00		PM 16:30 - 17:30		AM 08:00 - 09:00		PM 16:30 - 17:30	
	Arrivals	Departures	Arrivals	Departures	Arrivals	Departures	Arrivals	Departures
Car	0	0	5	1	0	0	1	0
Large Goods Vehicle	0	0	2	2	0	0	0	0
Pedal Cycle	0	0	0	0	0	0	0	0
Pedestrian	0	0	0	0	0	0	0	0
Motorcycle	0	0	0	0	0	0	0	0
OGV 2	0	0	0	0	0	1	0	0

The data shows that during the observed AM period no movements to or from the site were recorded, with one farm vehicle (classified as OGV 2) egressing the Bolton Fold Farm access road, using the access junction.

In the PM period 10 two-way movements were observed along the site access road, with one car entering the Bolton Fold Farm aspect of the wider site.

During the site visit it was also noted that three vehicles were located at the site prior to the AM traffic counts and remained at the site for the duration of the observations.

In the full survey data appended, there are shown to be 34 and 36 two-way vehicle movements on Alston Lane in the AM and PM peak hour periods, which equates to approximately just over one vehicle per minute in both directions.

Additionally, prior to the PM data collection it was noted that a total of four additional vehicles had accessed the site, where a total of seven vehicles were located at the development. Therefore, the traffic survey observed a maximum parking accumulation at the site of 11 vehicles in the PM period. It is therefore considered that the parking provision would comfortably cater for the level of demand associated with the development.

### **4.3 Trip Generation Summary**

Overall, based on an assessment of the operational application site it has been determined that the commercial units result in negligible multi-modal trips, and therefore, have no material impact on the operation of the local highway network.

## 5 Summary and Conclusions

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Dynamic Transport Planning is instructed by Mr N Forshaw to report on the anticipated highways and transportation considerations related with a retrospective application for the change of land-use of existing agricultural buildings to B2/B8, E-Class uses and *Sui Generis* on land at Bolton Fold Farm, Alston Lane, Longridge. The GFA of the existing units will be retained, where the external buildings will not be altered.

A review of the highway accident data for the site access and adjacent highway has been reviewed and identified no pre-existing patterns or trends of incidents.

A swept-path assessment has been undertaken which demonstrates that the largest anticipated service vehicle can comfortably be accommodated within the site, with servicing and deliveries to reflect those previously associated with the site's extant use.

A review of the site's accessibility has identified that the site has the potential to be accessed by sustainable modes of travel, which would work to encourage access by sustainable modes, for both staff and customers.

The trip generating potential of the development was assessed using information provided by the client regarding the daily operation of the site, combined with the multi-modal movements observed during a site visit. Overall, it was concluded that the proposed change of land-use would result in negligible multi-modal trips, with 10 two-way movements in the PM peak hour.

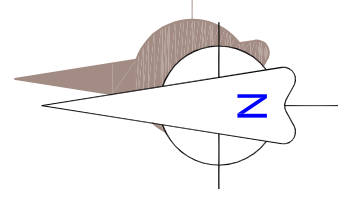
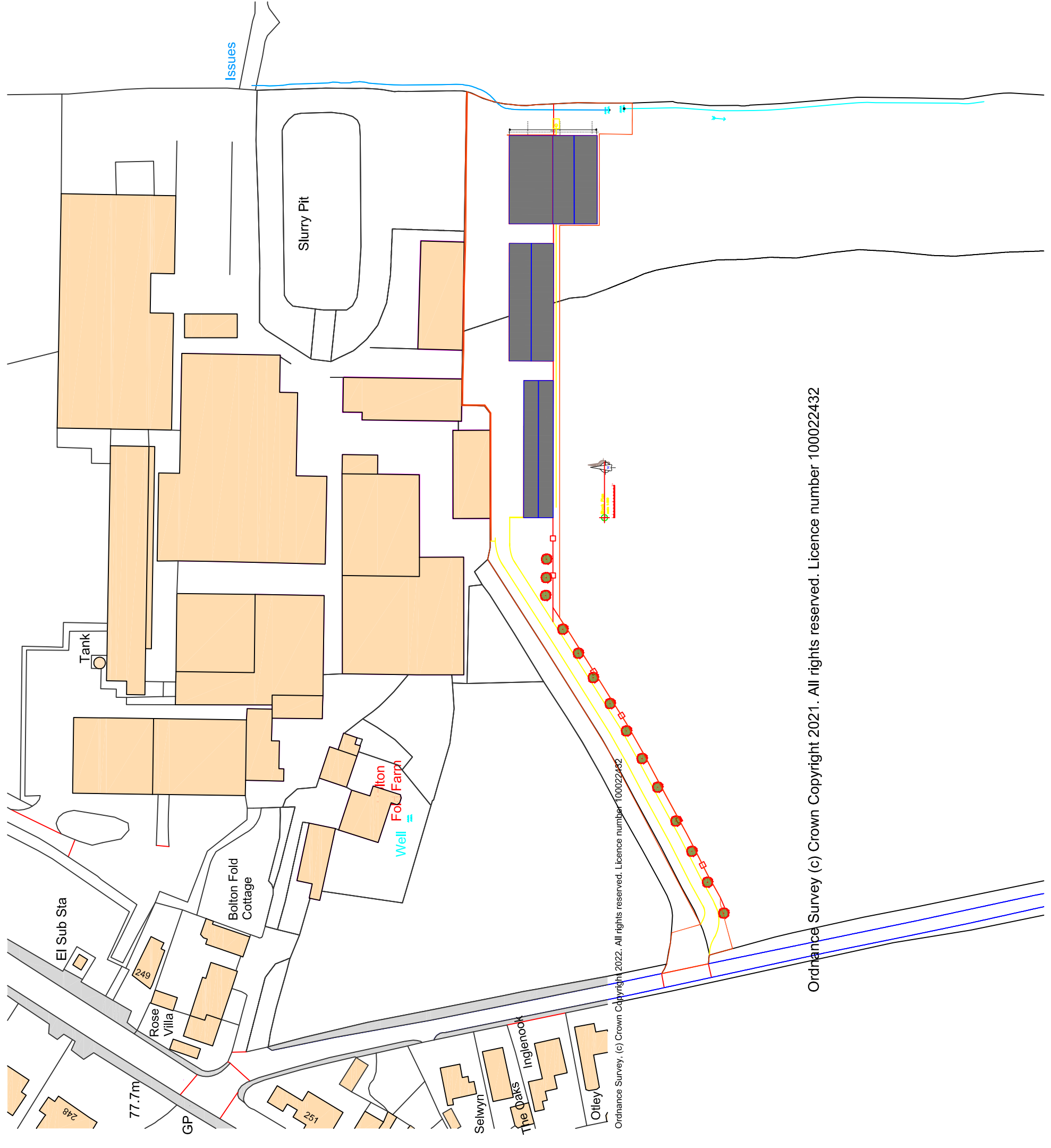
Overall, on the basis of the above assessment it is concluded that the change of land-use would not have a material impact on the local highway network and there are no outstanding reasons why the application should not be granted approval on highways grounds.

# Appendix 1

## Proposed Site Layout

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NOTES  
 THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS FOR THIS PROJECT. ALL LEVELS AND DIMENSIONS ARE TO BE CHECKED ON SITE BY THE CONTRACTOR, PRIOR TO PLACING ORDERS OR COMMENCING ANY OF THE RELEVANT WORKS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE INFORMATION SHOWN ON THE PLAN AND THAT FOUND ON ANY DISCREPANCIES BETWEEN THE INFORMATION SHOWN ON THE PLAN AND THAT FOUND ON SITE ARE TO BE REPORTED TO THE DESIGNER IMMEDIATELY.



A 10.05.22 amendments for Planning Consultant

Rev.	Date	Description

**A.L.H. Design Services**  
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 PR3 3NB, Tel & Fax : (017772) 785719

Client

Mr N Forshaw,  
 Bolton Fold Farm,  
 Alston Lane, Alston,  
 Longridge,  
 Nr Preston, Lancs, PR3 3BN

Job/Scheme Title

3 No NEW COMMERCIAL UNITS

Drawing Title

PROPOSED LOCATION PLAN

Job/Scheme Number	Drawing Number
0616193	Drawing No 000A
Scale	Date
1/1250	May 21
	a.t.l-h.
	Drawn
	Checked

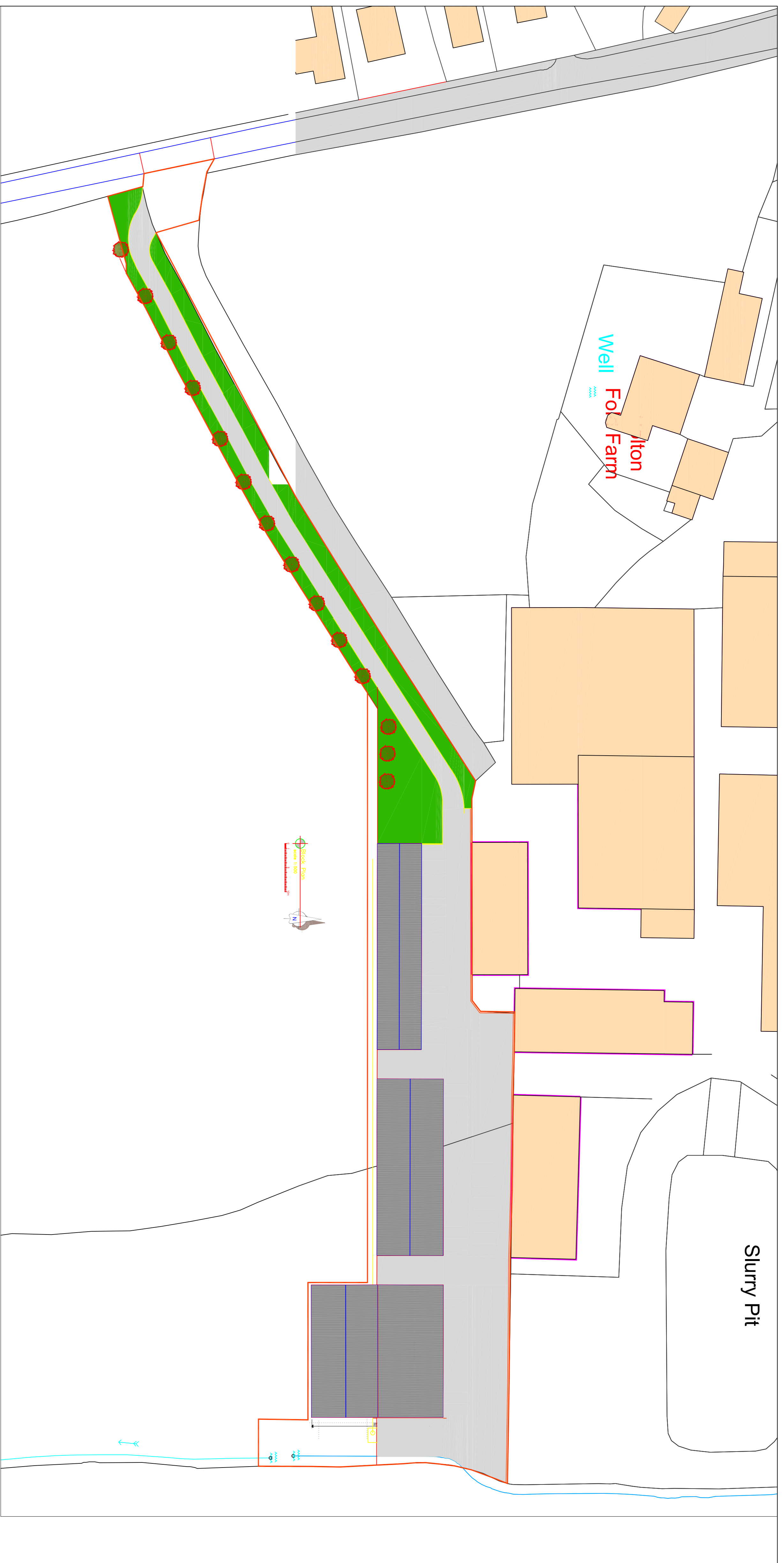
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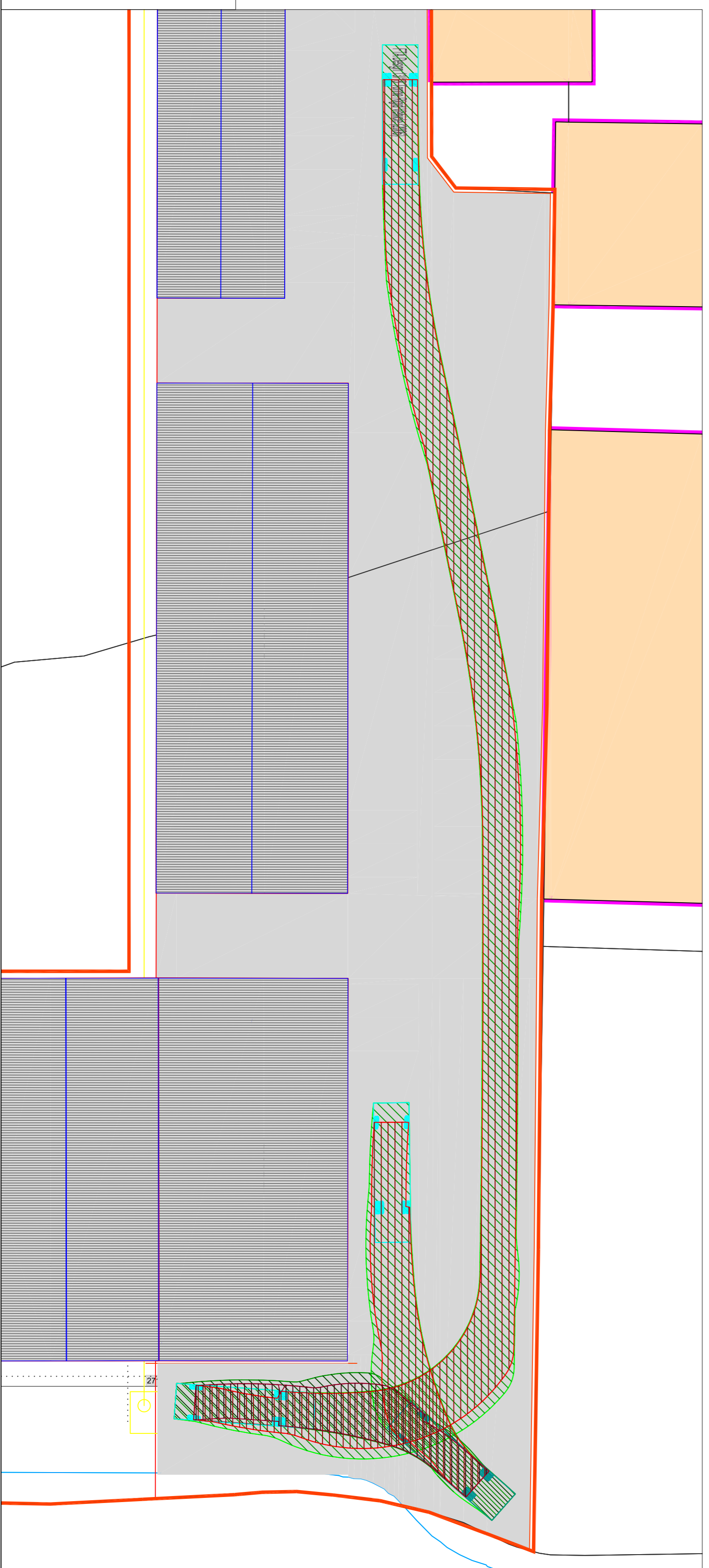
# Appendix 2

## Swept-Path Assessment

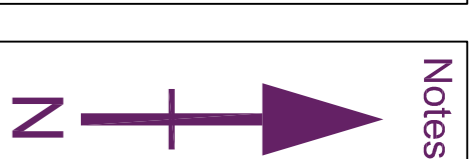
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Proposed Site Layout Plan - Scale 1/500

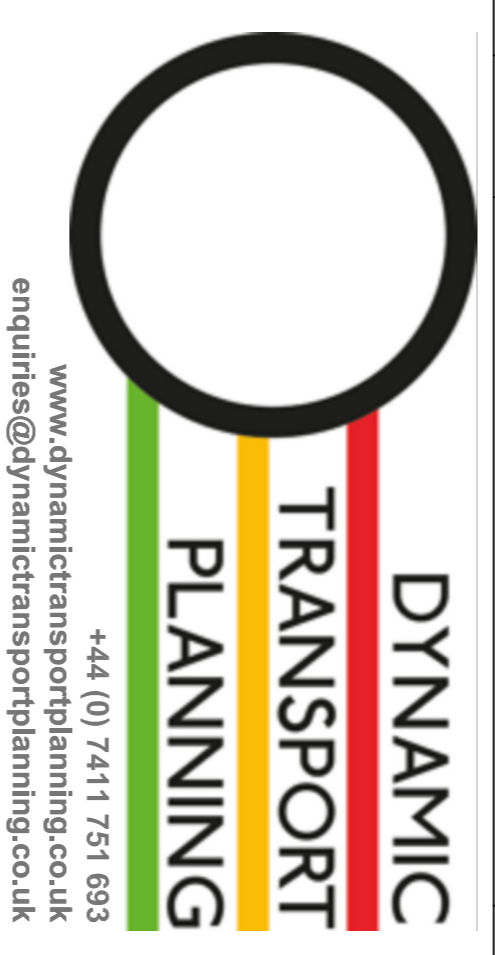


Swept Path Analysis - 10m Rigid - Scale 1/250



Notes

Rev.	Date	Revisions	Drawn
B	04/08/23	Minor Site Layout Plan Update	BF
-	17/07/23	First Issue	BF



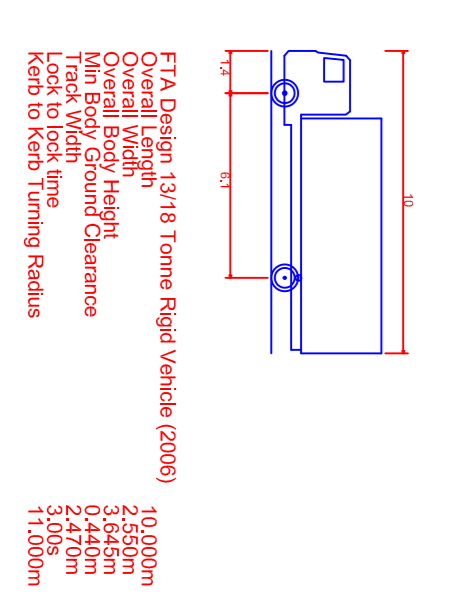
Client

Mr N Forshaw

Project  
Proposed Change of Land-Use,  
Bolton Fold Farm, Alston Lane,  
Longridge

Drawing Title  
Proposed Site Layout  
Swept Path Analysis  
10m Rigid

Date	Scale
August 2023	As Specified @ A1
Drawn BF	Approved JB
Drawing Number DTP/3707323/ATR001	Rev. B



# Appendix 3

## Traffic Count

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<b>Site:</b>	<b>Bolton Fold Farm, Longridge</b>	<b>A:</b>	<b>Alston Lane (N)</b>
<b>Day:</b>	<b>Thursday</b>	<b>B:</b>	<b>Farm Access</b>
<b>Date:</b>	<b>15 June 2023</b>	<b>C:</b>	<b>Units Access</b>
<b>Weather:</b>	<b>Sunny with No Cloud All Day</b>	<b>D:</b>	<b>Alston Lane (S)</b>

A - B									A-C								A-D							
Time	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total
08:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
08:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
08:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>

B-A									B-C								B-D							
Time	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total
08:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:30	0	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45	0	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>Total</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

C-A									C-B								C-D							
Time	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total
08:00	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:15	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
08:45	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>Total</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>

D-A									D-B								D-C							
Time	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	OGV2	P/C	M/C	PSV	Total



<b>Total</b>	<b>1</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>
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Time	D-A								D-B								D-C							
	Car	LGV	OGVI	0GV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	0GV2	P/C	M/C	PSV	Total	Car	LGV	OGVI	0GV2	P/C	M/C	PSV	Total
16:30	3	3	0	0	0	0	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
16:45	1	1	0	0	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:00	0	2	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
17:15	2	3	0	0	1	0	0	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>Total</b>	<b>6</b>	<b>9</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>17</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>