

Leagram Mill Farm, Leagram

TN01 – Response to Lancashire County Council Highways

Client:	Kim Weld-Blundell	Job No:	J329380
Date:	05 February 2026	File Name:	TN01
Prepared by:	LJ	Approved by:	ME

1. Introduction

1.1 Background

- 1.1.1 Mode Transport Planning ('Mode') has been appointed by Kim Weld-Blundell (the 'Applicant') to provide transport and highways advice in relation to a residential development at the barn and outbuilding adjacent to Leagram Mill Farm in Leagram.
- 1.1.2 A planning application (Ref: 3/2025/0895) was submitted to Ribble Valley Borough Council (RVBC) for the proposed conversion of an existing barn to create a "4-bedroom dwelling with detached dwelling with associated parking, landscaping and amenity space".
- 1.1.3 The Local Highway Authority, Lancashire County Council (LCC) were consulted on the application and provided a response dated 9th December 2025 that stated:
- 'Lancashire County Council acting as the Local Highway Authority does not consider that the application as submitted fully assesses the highway impact of the proposed development and further information is required as set out in this response..... the LHA has identified several matters that require further clarification and amendment before the applicant can be fully assessed. Including detailed visibility splays and the position of the gates at the site.'*
- 1.1.4 The full consultation response provided by LCC in response to the planning application is attached in [Appendix A](#).
- 1.1.5 This note (TN01) has been prepared in response to comments received from LCC Highways and provides evidence to address the concerns raised by LCC with particular consideration given to the proposed site access arrangement.

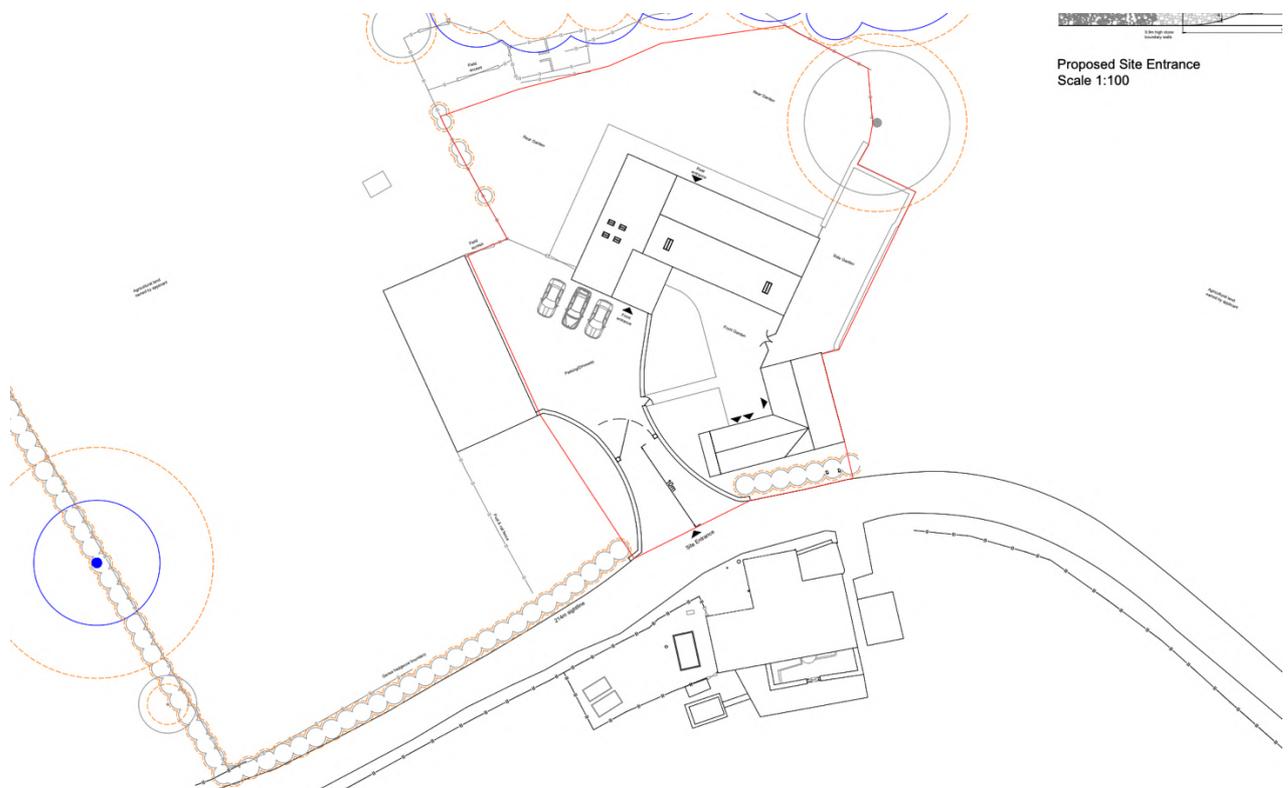
2. Development Proposals

2.1 Proposed Development

2.1.1 The development proposals include the conversion of an existing barn to create a “4-bedroom dwelling with detached dwelling with associated parking, landscaping and amenity space”.

2.1.2 The proposed site layout is shown in [Figure 2.1](#) and attached in [Appendix B](#).

Figure 2.1 : Proposed Site Layout (source: Zara Moon Architects)



3. Site Access Appraisal

3.1 Background

3.1.1 The site access proposals submitted as part of Ref: 3/2025/0895 proposed the use of an amended existing access on the northern side of the C567. The full consultation response from LCC Highways is attached in [Appendix A](#). The response includes comments regarding the proposed site access arrangement submitted as part of the planning application, to which LCC stated the following:

‘..... to ensure adequate intervisibility between highway users at a new access a visibility splay set 2.4 metres back from the near edge of the carriageway and extending 214 metres along the nearside carriageway edge in both directions should be provided.

The visibility splay should be wholly within the land which the applicant controls and/or the adopted highway and nothing shall be erected, retained, planted and / or allowed to grow at or above a height of 0.9 metres above the nearside carriageway level which would obstruct the visibility splay of the proposed access.

Or alternatively, reduced sight lines may be acceptable if the applicant can provide an observed traffic speed study, which indicates the 85th percentile traffic speeds are less than detailed above. The traffic study would need to be carried out by an approved specialist showing the 85th percentile wet road traffic speeds in both directions, the numbers of vehicles, types of vehicles and the speed of vehicles passing the access..... the proposed gates should be set further back into the site at 10m behind the carriageway edge. This is because the site will provide access to an agricultural building and as such, agricultural vehicles and trailers require further space to pull clear of the carriageway. The access shall be paved in a permeable hardstanding material for 10m into the site to prevent debris from being dragged onto the highway and drainage, collected within the site and drained to a suitable internal outfall, should be provided at the access point to prevent water from discharging onto the highway.'

3.2 Traffic Survey Data

3.2.1 An Automatic Traffic Counter (ATC) survey was carried out for 7 days by an independent traffic survey company from 19th to 25th January 2026. The ATC recorded two-way 85th percentile speeds in the vicinity of the site access. A summary of the results is provided in [Table 3.1](#), with the raw traffic survey data attached in [Appendix C](#).

Table 3.1 : ATC Results Summary – C567

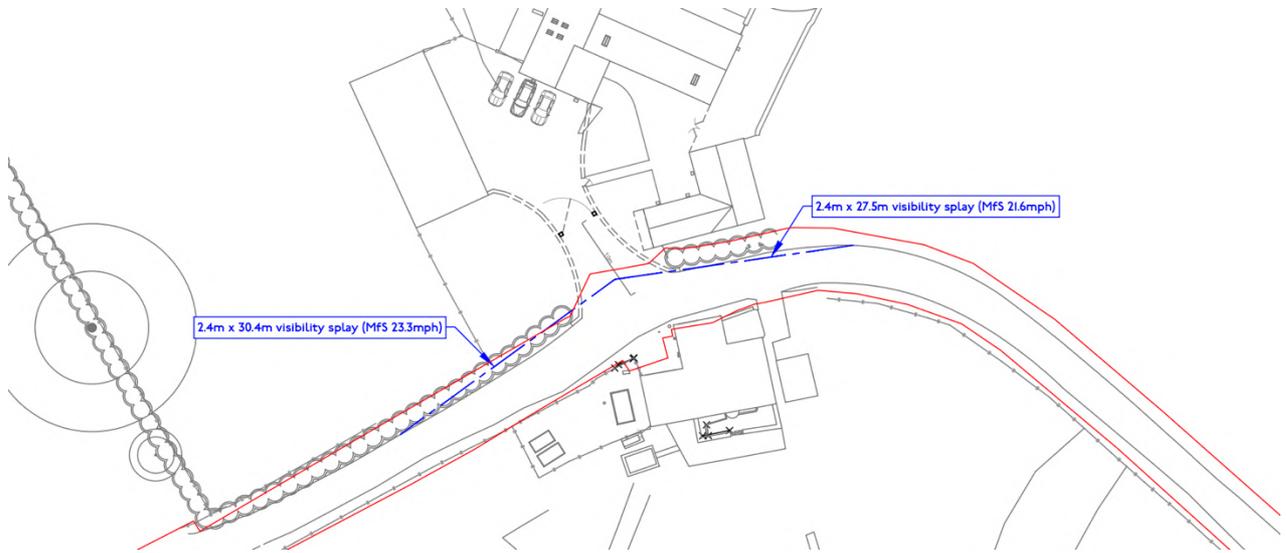
Direction	85 th Percentile Speeds
Eastbound	21.6 mph
Westbound	23.3 mph

3.2.2 Based on Manual for Streets (MfS) standards for the recorded 85th percentile speeds shown in [Table 3.1](#), visibility splays of 2.4m x 27.5m and 2.4m x 30.4m are required to the west and east respectively.

3.3 Proposed Access Arrangement

3.3.1 The proposed site access arrangement and required visibility splays for the recorded 85th percentile speeds are shown in [Figure 3.1](#), and in [Drawing J32-9380-PS-001](#) attached in [Appendix D](#).

Figure 3.1 : Proposed Site Access Arrangement



3.3.2 Based on MfS standards for the recorded 85th percentile speeds shown in [Table 3.1](#), visibility splays of 2.4m x 27.5m and 2.4m x 30.4m are achievable to the west and east respectively in land under the control of the applicant and the adopted highway. The landscaping to the west of the proposed access will not exceed 0.9m above the carriageway level. The proposed gate has been set back into the site at 10m from the carriageway edge.

3.4 Servicing and Deliveries

3.4.1 Servicing of the site would be undertaken from the adopted highway, as per the existing arrangement for the residential property opposite.

3.4.2 Deliveries to the proposed development would be undertaken using the proposed access and therefore, swept path analysis has been undertaken for a 7.5t panel van, which is the largest regular delivery vehicle expected to enter the site. This is shown in [Figure 3.2](#), attached in **Drawing J32-9380-PS-001** in [Appendix D](#).

Figure 3.2 : Swept Path Analysis – 7.5t Box Van



3.4.3 As shown in Figure 3.2, a delivery vehicle can enter and exit the site in forward gear and manoeuvre safely within the site.

3.5 Proposed Parking

3.5.1 As noted in the consultation response, LCC indicated that the total level of car parking was acceptable.

3.5.2 The proposed development will provide an Electric Vehicle Charing Point (EVCP). The proposed development will also provide a secure covered cycle store to encourage the use of sustainable transport.

4. Summary

4.1.1 The planning application (Ref: 3/2025/0895) seeks approval for the conversion of an existing barn to create a “4-bedroom dwelling with detached dwelling with associated parking, landscaping and amenity space”. Access to the site is proposed via an amended existing access on the northern side of the C567

4.1.2 Lancashire County Council (LCC), as the Local Highway Authority, responded to the consultation on 9th December 2025, stating that there were several matters that required further clarifications and amendments before the application could be determined. They requested detailed visibility splays based on 85th percentile speeds and the positioning of the gates at the site access.

4.1.3 This note addresses the concerns raised by LCC, particularly regarding the proposed site access. The evidence presented in this note confirms that the access has been designed in accordance with the relevant highway design guidance and visibility standards.

APPENDIX A - LCC Highways Consultation Response

Ribble Valley Borough Council
Council Offices
Church Walk,
Clitheroe
Lancashire
BB7 2RA

Phone: 0300 123 6780
Email: developeras@lancashire.gov.uk
Your ref: 3/2025/0895
Our ref: 3/2025/0895/HDC/KW
Date: 09 December 2025

Location: Barn and outbuilding adjacent to Leagram Mill Farm Dinkling Green Lane Chipping PR3 2QS
Proposal: Conversion of existing barn and outbuilding to create a 4 bedroom dwelling with detached dwelling, including alterations to fenestration, insertion of rooflights, insertion of flue and associated landscaping, parking and private amenity space.
Grid Ref: 363862 444145

Dear Maya Cullen

With regard to your consultation letter dated 21 November 2025, I have the following comments to make based on all the information provided by the applicant to date.

Summary

Further Information

Lancashire County Council acting as the Local Highway Authority (LHA) does not consider that the application as submitted fully assesses the highway impact of the proposed development and further information is required as set out in this response.

Advice to Local Planning Authority

Introduction

The Local Highway Authority (LHA) are in receipt of an application for the proposed conversion of existing barn and outbuilding to create a 4 bedroom dwelling with detached dwelling, including alterations to fenestration, insertion of rooflights, insertion of flue and associated landscaping, parking and private amenity space at Barn and outbuilding adjacent to Leagram Mill Farm, Dinkling Green Lane, Chipping.

Site Access

The site will be accessed via an amended existing access onto Dinkling Green Lane, classified as the C567, subject to a national speed limit. The access amendments include widening the existing access point to approximately 11m, the placement of a boundary wall around the access and gates set back approximately 9.5m.

The site access will also provide access to the existing agricultural building with recent planning permission granted to replace the building with a new agricultural building under

Continued...

reference 3/2025/0387, as well as retain the existing access into an agricultural field to the west of the site.

The proposal will generate more car movements and drivers will not have the same clearance over vegetation as the existing agricultural vehicles using the site. As such to ensure adequate intervisibility between highway users at a new access a visibility splay set 2.4 metres back from the near edge of the carriageway and extending 214 metres along the nearside carriageway edge in both directions should be provided.

The visibility splay should be wholly within the land which the applicant controls and/or the adopted highway and nothing shall be erected, retained, planted and / or allowed to grow at or above a height of 0.9 metres above the nearside carriageway level which would obstruct the visibility splay of the proposed access.

Or alternatively, reduced sight lines may be acceptable if the applicant can provide an observed traffic speed study, which indicates the 85th percentile traffic speeds are less than detailed above. The traffic study would need to be carried out by an approved specialist showing the 85th percentile wet road traffic speeds in both directions, the numbers of vehicles, types of vehicles and the speed of vehicles passing the access.

The applicant should provide accurate details of the required sight line requirement, before determining the application.

Further amendments are required in regard to the access; the proposed gates should be set further back into the site at 10m behind the carriageway edge. This is because the site will provide access to an agricultural building and as such, agricultural vehicles and trailers require further space to pull clear of the carriageway. The access shall be paved in a permeable hardstanding material for 10m into the site to prevent debris from being dragged onto the highway and drainage, collected within the site and drained to a suitable internal outfall, should be provided at the access point to prevent water from discharging onto the highway.

Internal Layout

The LHA have reviewed drawing number 07 titled Proposed Site Plan and are aware that the dwelling complies with the LHAs parking standards as defined in the Joint Lancashire Structure Plan.

There is room to manoeuvre within the site which will allow for ingress and egress in a forward gear.

Public Right of Way

Please note that there is a Public Rights of Way FP0310009 close to the application site which must not be obstructed during the proposed developments. It is the responsibility of the landowner to ensure that the necessary procedures are followed for the legal diversion of the Public Right of Way if this should be necessary. The granting of planning permission does not constitute the diversion of a Definitive Right of Way.

Drainage

For drainage strategies which are connecting to a watercourse, the Applicant needs to be aware that under the Land Drainage Act 1991 consent is required from the Lead Local

Flood Authority for work which may alter or impede the flow of water, regardless of whether the watercourse is culverted or not. Consent must be obtained before works are started on site as it cannot be issued retrospectively.

Sustainability

Given the site's distant location from local amenities and facilities, including public transport, there would be a reliance on the use of private motor vehicles.

The site shall enhance its sustainable transport options to actively encourage and support sustainable transport use. The development must include covered and secure cycle storage to ensure adequate parking provisions and to promote cycling as a sustainable mode of transport. The LHA note the presence of a cycle and bin store provided adjacent to the outbuilding.

In addition, given the nature of the application, the development is expected to incorporate at least one electric vehicle (EV) charging point to further encourage sustainable travel choices. In line with Department for Transport (DfT) guidance on Electric Vehicle Charging in Residential and Non-Residential Buildings, charge points must deliver a minimum power output of 7 kW and be equipped with a universal socket capable of charging all types of electric vehicles.

Conclusion

To conclude, the LHA has identified several matters that require further clarification and amendment before the application can be fully assessed. Including detailed visibility splays and the positioning of the gates at the site access.

Yours sincerely

Kate Walsh

Assistant Engineer

Highway Development Control

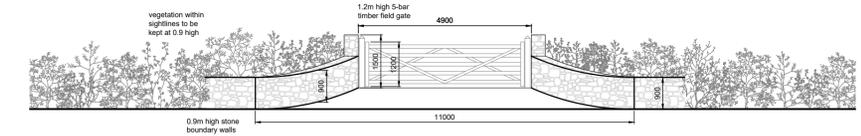
Highways and Transport

Lancashire County Council

T: 0300 123 6780

W: <http://www.lancashire.gov.uk>

APPENDIX B - Proposed Site Layout



Proposed Site Entrance
Scale 1:100

Proposed Site Plan
Scale 1:200

Notes

This drawing, together with the design it illustrates, is copyright, and may not be reproduced in any form, without the written consent of Zara Moore Architects.

Do not scale off this drawing except for determination of basic layout - only figure dimensions to be used. All dimensions to be checked on site.

Key

- Application Boundary
- Category B Trees
- Category C Trees
- Root Protection Area

ZMA
Zara Moore Architects

RIBA Chartered Practice

Project Name Leagram Mill Farm	Project No. 210.24	Stage Planning Submission
Proposed Site Plan 07	Scale 1:200 @ A1	Date September 2025
Rev. A	Drawn	RP

Proposed site entrance elevation section
Date

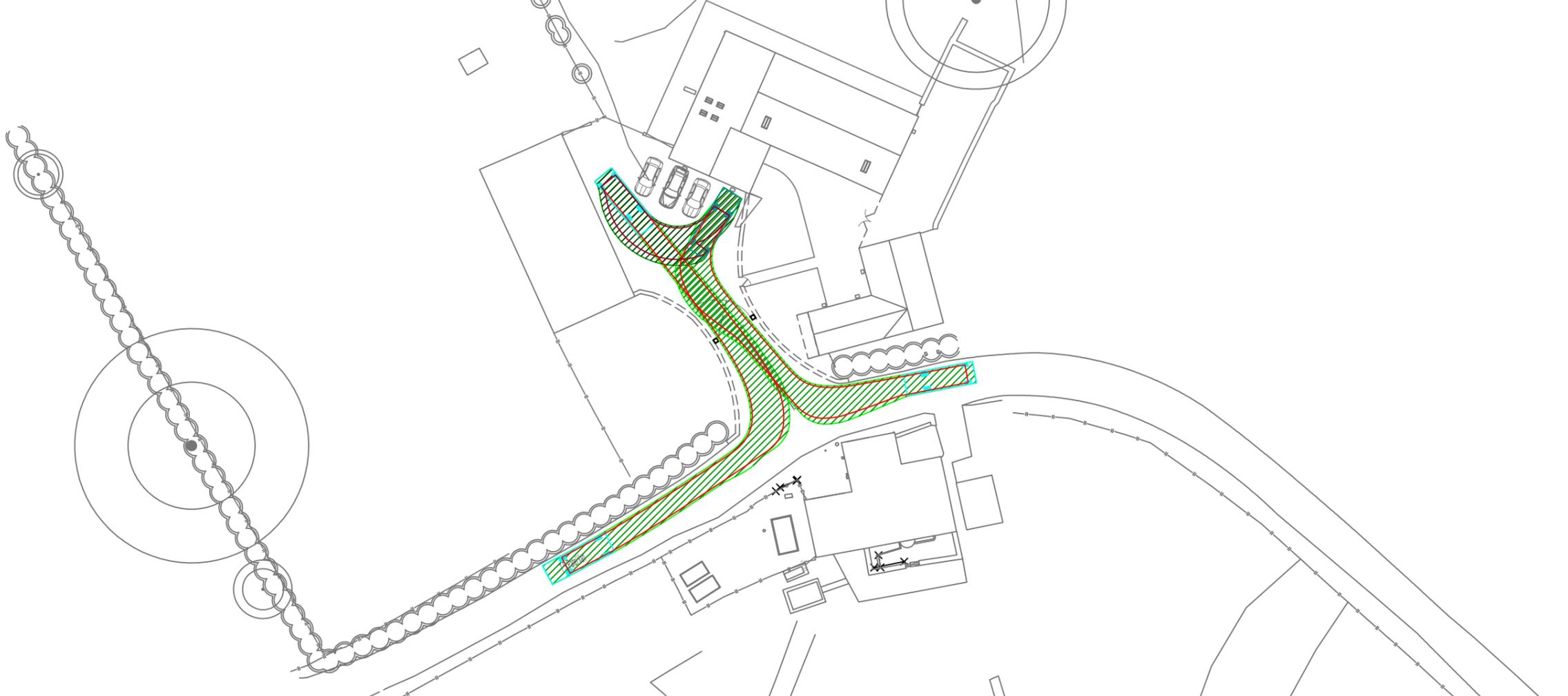
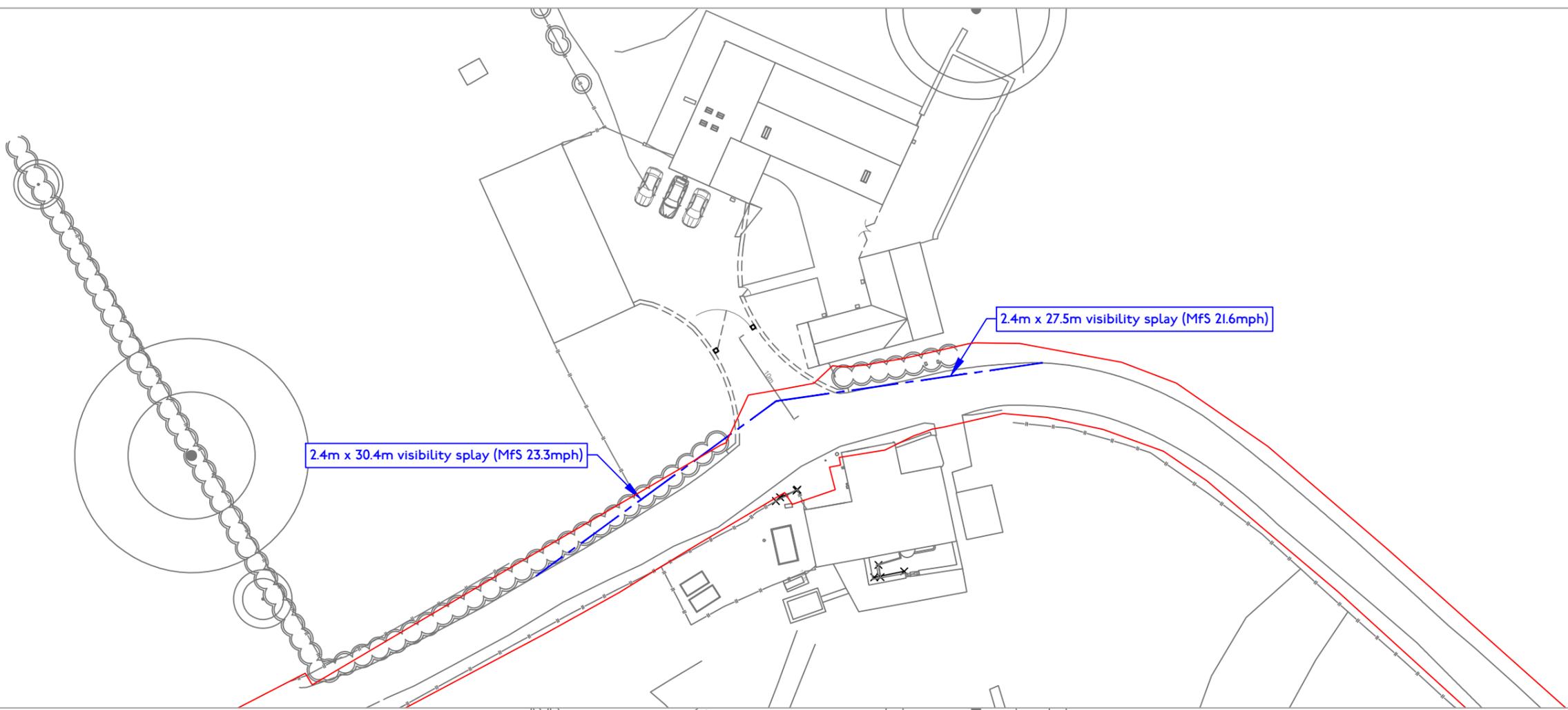
APPENDIX C - Traffic Survey Data

APPENDIX D - Proposed Site Access and Swept Path Analysis

File name: C:\USERS\WINDOWS\VIDE DROPBOX\PROJECT\MANCHESTER\PROJECTS\J329380_LEAGRAM MILL FARM LEAGRAM.MXS GRAPHICS\2 CAD\1 XREFS\X_LEAGRAM MILL FARM - PROPOSED SITEDWG
 Last saved by: WINDOWS Last Picked: 2026-02-05

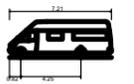
Project Management Initials: Designer: LCM Checked: MA Approved: ME

ISO A3 297mm x 420mm



transport planning

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 w www.modetransport.co.uk



7.5t Panel Van	7.210m
Overall Length	2.192m
Overall Width	2.344m
Overall Body Height	0.316m
Min Body Ground Clearance	1.965m
Track Width	4.05m
Lock to lock time	7.400m
Kerb to Kerb Turning Radius	

notes:

1. this drawing is to be read in conjunction with all other relevant drawings, any discrepancies, errors or omissions to be brought to the attention of overseeing organisation.
2. all dimensions to be checked before commencement of work on site.
3. all dimensions in metres unless otherwise stated.
4. the design is subject to approval of lancashire county council.
5. drawing based on topographical survey.

issue/revision

i/r	date	issued	description
-	05/02/2026		

client: kim weld-blundell
 project: leagram mill farm, leagram
 project number: J329380
 scale: 1:500@A3
 drawing title:
 preliminary site access
 & swept path analysis
 drawing number:
 J32-9380-PS-001